



March 26, 2025

Aaron Bert  
Director of Public Works  
City of Redmond  
15670 NE 85<sup>th</sup> Street  
Redmond, WA 98073

**Re: Letter of Concurrence between Sound Transit and City of Redmond  
Regarding RCC Trail Improvement Scope Modifications**

Mr. Bert,

As you know, pursuant to that certain “Agreement Between City of Redmond and the Central Puget Sound Regional Transit Authority for the Funding, Design, and Construction of Downtown Redmond Link Extension Betterments” dated October 18, 2019 (the “Betterment Agreement”), attached hereto as Attachment 1, Sound Transit agreed to design and construct improvements to the Redmond Central Connector Trail (the “RCC Improvements”) subject to certain cost sharing between Sound Transit and the City. Exhibit D to the Betterment Agreement included the conceptual design for the RCC Improvements, including specified plantings and finish levels.

Pursuant to that certain “Light Rail Easement Agreement (Redmond Spur Rail Corridor—Downtown City Segment)” dated April 11, 2012 (the “Sound Transit Easement”), Sound Transit acquired the right to construct, operate, and maintain its light rail transit facilities for the Downtown Redmond Link Extension Project (the “Project”) on certain City property. The Sound Transit Easement property includes property upon which Sound Transit is to construct the RCC Improvements under the Betterment Agreement.

PPF AMLI Redmond Way, LLC (AMLI), the owner of property adjacent to the portion of the Sound Transit Easement located between 170<sup>th</sup> Avenue Northeast and Downtown Redmond Station and upon which a portion of the RCC Improvements are to be constructed, now desires to use a portion of the Sound Transit Easement area (the “Construction Access Area”), referred to as the “License Area” in the AMLI Letter of Agreement referenced below, for construction staging and access and such other purposes as agreed to by the City (the “Permitted Activities”) and the City desires to allow same.

Accordingly, the City has requested that a portion of the RCC Improvements be finished to a level appropriate for AMLI’s use for completion of the Permitted Activities instead of being finished to the level identified in the Betterment Agreement as set forth in the June 25, 2024 City letter attached hereto as Attachment 2 (the RCC Scope Modification

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**INTERIM  
CHIEF EXECUTIVE OFFICER**  
**Goran Sparrman**

Request Letter). Sound Transit agrees to the proposed change in finish level obligations and to amend the Betterment Agreement in the form attached hereto as Attachment 3 to reflect the same, subject to the following conditions:

1. The City will be responsible for ensuring compliance with the requirements in this Letter of Concurrence. The City shall ensure that the RCC is restored, at no responsibility or cost to Sound Transit, as described in the RCC Scope Modification Request Letter, attached hereto as Attachment 2, and permit number SITE-2023-02147 as revised on July 22, 2024 to satisfy the requirements of the modified Betterment Agreement within the Construction Access Area.
2. Prior to entering the Sound Transit Easement area, AMLI must sign the Letter of Agreement attached hereto as Attachment 4 (the “AMLI Letter of Agreement”), authorizing AMLI’s use of the Construction Access Area<sup>1</sup> pursuant to the terms and conditions therein.
3. The City accepts the Construction Access Area in the condition described in the RCC Scope Modification Request Letter, attached hereto as Attachment 2, including accepting all work performed in the Construction Access Area by Sound Transit for its light rail transit facilities. The City will communicate acceptance of the Construction Access Area condition when the area is transferred to the City via the closeout of Site Civil Permits for the limits of the Construction Access Area.
4. The City will identify and protect, or cause to be identified and protected, all third party facilities including, but not limited to, Puget Sound Energy lines and ductbanks, temporary irrigation, and storm and sewer lines prior to and during the work taking place in the Construction Access Area. The City will be responsible for protection of and damage to all such facilities within the limits of the Construction Access Area.
5. The City will take ownership of and responsibility for the concrete modules. Sound Transit will deliver the concrete modules to a mutually agreed upon location in the Construction Access Area.
6. The City is responsible for trail and road diversions associated with AMLI's use of the Construction Access Area, including communication and coordination with Sound Transit.
7. Upon the City and Sound Transit amending the Betterment Agreement and Sound Transit allowing AMLI to access the Construction Access Area, the City: (1) acknowledges that all of Sound Transit’s requirements in the Betterment Agreement relating to the RCC Improvements located within the Construction Access Area shall have been conclusively fulfilled; (2) shall permanently exclude those requirements from any permits required by the City for the DRLE Project; and (3) shall provide a written release or such other documentation of same as Sound Transit may reasonably require.
8. The City agrees to defend, indemnify, and hold harmless Sound Transit and its officers, directors, agents, and employees (the “Indemnified Parties”) from and against any and all claims, demands, or causes of action and the resulting losses, costs, expenses, reasonable attorney’s fees, liabilities, damages, orders, judgments, or decrees (“Claims”) arising out of the acts, errors, or omissions of the City or its Contractors related to or in any way arising out of the City’s or its Contractors’ use of the Construction Access Area or the City’s breach of this Letter of Concurrence. To the extent that RCW 4.24.115 applies, the indemnity provisions provided herein shall be valid and enforceable only to the extent of the negligence of the City and/or its Contractors and shall not apply in the event that any Claims arise out of Sound Transit’s sole negligence. The City specifically assumes potential liability for any claim, demand, and/or cause of action brought by, or on behalf of, any of its employees or agents against the Indemnified Parties. FOR THIS

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<sup>1</sup> The “Construction Access Area” identified in this Letter of Concurrence is the same area identified in the AMLI Letter of Agreement as the “License Area” (labeled in red in Exhibit A to the AMLI Letter of Agreement as the “Construction Access Area”).

PURPOSE, THE CITY, BY MUTUAL NEGOTIATION, HEREBY WAIVES, WITH RESPECT TO THE INDEMNIFIED PARTIES ONLY, ANY IMMUNITY THAT WOULD OTHERWISE BE AVAILABLE TO THE CITY AGAINST SUCH CLAIMS UNDER THE INDUSTRIAL INSURANCE PROVISIONS OF TITLE 51 RCW OR ANY APPLICABLE INDUSTRIAL INSURANCE, DISABILITY, OR EMPLOYEE BENEFIT ACT OF ANY JURISDICTION THAT WOULD BE APPLICABLE IN CASE OF SUCH A CLAIM.

9. In addition to the foregoing indemnities, the City agrees to protect, defend, and indemnify and hold the Indemnified Parties harmless for any Claims associated with the presence, removal, or remediation of any Hazardous Substance (including petroleum and gasoline products) that are released onto or from the Construction Access Area, or otherwise come to be located on the Construction Access Area as a result of the City or its Contractors' use of the Construction Access Area. "Hazardous substances," for purposes of this section, include, but are not limited to, those substances included within the definition of "hazardous substances," "hazardous materials," "toxic substances," "hazardous wastes" or solid wastes in any federal, state, or local law, statute, ordinance, regulation, order, or rule pertaining to health, industrial hygiene, environmental conditions, or hazardous substances. "Costs" shall include, but not be limited to, all response or remediation costs, disposal fees, investigation costs, monitoring costs, civil or criminal penalties, attorney's fees, and other litigation costs incurred in connection with such response or remediation.
10. Sound Transit agrees that once AMLI has signed the Letter of Agreement pursuant to the terms of Paragraph 2 of this Letter of Concurrence, the conditions of Paragraphs 3(b), 3(d), 4(a), and 8(a)-(b) of the Letter of Agreement are deemed satisfied for the activities permitted under the terms of Redmond SITE-2023-02147 within the Construction Access Area.
11. Based on the current understanding of the scope of work included in this Letter of Concurrence, the parties acknowledge that all costs can be addressed under the current betterment agreement and there are no necessary changes to the cost provisions therein.

Should you have any questions, please don't hesitate to contact me at (206) 398-5171. Please sign below and return one of the enclosed originals – please keep the additional copy for your records.

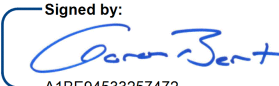
Sincerely,

Tony Raben  
Executive Project Director, DRLE

Approved as to form:

Sound Transit Senior Legal Counsel

Concurrence:

Signed by:  
  
A1BE94533257472...  
Aaron Bert, Director of Public Works  
City of Redmond

Attachments:

1. GA 0143-19 Betterment Agreement
2. RCC Scope Modification Request Letter
3. DRAFT Amendment 1 to GA 0143-19 Betterment Agreement
4. Letter of Agreement for AMLI Construction Staging Rights



**AGREEMENT  
BETWEEN CITY OF REDMOND AND THE CENTRAL PUGET SOUND  
REGIONAL TRANSIT AUTHORITY  
FOR THE FUNDING, DESIGN, AND CONSTRUCTION OF  
DOWNTOWN REDMOND LINK EXTENSION BETTERMENTS**

**GA 0143-19**

THIS AGREEMENT, effective upon the date of the latest signature, is entered into by and between the CITY OF REDMOND, a Washington optional municipal code city (the "City") and the CENTRAL PUGET SOUND REGIONAL TRANSIT AUTHORITY, a regional transit authority organized under the laws of the State of Washington ("Sound Transit"), collectively "Parties" and individually "Party."

**RECITALS**

A. The Sound Transit Regional Transit Long-Range Plan identifies the Downtown Redmond Link Extension Project ("Project") within the City's jurisdiction and was funded as part of the 2016 voter-approved regional transit system plan, Sound Transit 3 Plan, ("ST3").

B. The Project will add two new light rail stations beyond the Redmond Technology Station (currently called Overlake Transit Center) with one station and 1,400 parking stalls at Southeast Redmond and the other in Downtown Redmond.

C. The Parties entered into the Development Agreement for the Downtown Redmond Link Extension Project on Aug. 21, 2019 to address the development standards for the Project ("Development Agreement"). In addition to the development standards, the Development Agreement identified certain public facilities to be included as part of Sound Transit's delivery of the Project.

D. Sound Transit will deliver the Project as a design-build procurement ("Design Build") and has been coordinating with the City in preparation for Project design and construction. As part of that coordination the City and Sound Transit have identified improvements to City infrastructure that are not required to execute the construction of Project, but would provide a public benefit and promote an efficient use of public funds if included as part Sound Transit's Design Build scope of work ("Betterments").

E. Sound Transit issued the DRLE Design-Build Request for Proposals ("DRLE RFP") in November 2018. Following the issuance of the DRLE RFP, Sound Transit engaged the City to inform the design of the Betterments. Sound Transit estimates that it will enter into the DRLE Project Design Build Contract ("DRLE Contract") with its Design-Build contractor (the "Design Builder") and issue the Notice to Proceed under that contract in September 2019.

F. The Parties desire to enter into this Agreement to set forth their respective responsibilities, define the City's funding commitment for the Betterments, develop a protocol for sharing information in a timely manner and at stages appropriate to ensure adequate consideration

of issues raised by the Parties, and decision-making standards that maximize transparency and efficiency in decision-making and build effective cooperation between the Parties.

In consideration of the recitals, terms, conditions, and covenants contained herein:

## **IT IS MUTUALLY AGREED AS FOLLOWS:**

### **SECTION 1 GENERAL**

**1.1 Incorporation of Recitals.** The above recitals are incorporated as part of this Agreement.

**1.2 Cooperation of the Parties.** The provisions contained in this Agreement depend upon timely and open communication and cooperation between the Parties. In this regard, communication of issues, changes, or problems that arise should occur as early as possible in the process. Each Party will work cooperatively and in good faith toward resolution of issues in a manner that ensures adequate time for each Party to consider and address the issues. This Agreement contemplates the execution and delivery of a number of future documents, instruments, and permits, the final form and contents of which are not presently determined. The Parties will provide the necessary resources and work in good faith to develop in a diligent and timely manner the final form and content of such documents, instruments, and permits.

### **SECTION 2 SCOPE OF WORK**

**2.1** In accordance with the terms of this Agreement, Sound Transit agrees to include in its DRLE Contract and the City agrees to pay all costs to perform the work described below. In instances where the Parties will share costs, the City agrees to pay its pro rata share of all costs associated with that Betterment. The Parties acknowledge that, due to the nature of design-build contracting, the Design Builder will complete the design for the Betterments in a manner consistent with the general scope described below, pursuant to the DRLE Contract.

- (a) NE 51st Street PRV and Water Main ("PRV"),** fully described in Exhibit A. Generally, Sound Transit will design and construct an additional 175 feet of 12" water main along with required appurtenances to support the installation of a new 8" pressure reduction valve (PRV) station and vault for the City.
- (b) NE 70th Street & State Route 202 ("70th Street"),** fully described in Exhibit B. Generally, Sound Transit will design and construct an additional northbound left turn lane on SR 202 at the intersection of SR 202 and NE 70th Street; a widened SR 202 to the northeast to accommodate the additional left turn lane; a realignment of the through lanes along SR 202 east and west of NE 70th Street required because of the additional left turn lane; modified storm water and drainage facilities required to accommodate the additional left turn lane; and modified traffic signals and lighting for the additional left turn lane.

- (c) **NE 76th Street & State Route 202** (“76th Street”), fully described in Exhibit C. Generally, Sound Transit will design and construct an additional eastbound left turn lane and northbound receiving/through lane on NE 76th St from SR 202 to the traffic signal serving Fred Meyer and Target; a new curb, gutter, pavement, and lane restriping on NE 76th Street from SR 202 to the Fred Meyer/Target traffic signal, a total distance of about 1,100 feet; remove the sidewalk on the existing SR 202 bridge over Bear Creek; a revised curb taper on SR 202; a new curb and sidewalk on the south side of SR 202 near Creekside Crossing; and a new channelization from the existing SR 202 bridge to the driveway of Creekside Crossing.
- (d) **Redmond Central Connector Improvements** (“RCCI”), fully described in Exhibit D. Generally, Sound Transit will design and construct improvements to the Redmond Central Connector, which will include improvements to the corridor beyond the scope of this Agreement that are the sole financial responsibility of Sound Transit. The RCCI Betterment will consist of all improvements to the Redmond Central Connector east of 166th Avenue NE, except for landscaping. The Parties will share the costs of the RCCI equally.

## **2.2 Change Process.**

**2.2.1 City Requested Changes.** Sound Transit has selected a design-build project delivery method for the Project. Because of the nature of a design-build contract, changes to the work that occur after contract award are strongly discouraged. If the City has an interest in changing the confirmed Betterment work, Sound Transit will consider such requests based on their anticipated impact to Project scope, schedule, budget, and long-term operations and maintenance costs. After consultation with the City, Sound Transit in its sole discretion will determine whether to approve such a change and the associated adjustments and impacts to Project work, schedule, budget, operations, and costs.

**2.2.2 Change Orders.** Any changes related to design or construction of the Betterments will be provided to the City for review and approval. If the City fails to respond within seven (7) days or other such time as the Parties agree in writing, Sound Transit may proceed with that specific change if Sound Transit determines that it is in the best interest of the Project. The City will not unreasonably withhold approval of any change. Sound Transit may request verbal approval of any change order from the City and may request to proceed with the work immediately in order to avoid delay cost, and the City may grant such verbal approval. Any such action will be documented immediately and written confirmation of approval will be obtained from the City if necessary. If a changed condition is found or the Design Builder requests an adjustment that increases the cost or time of performance, Sound Transit will consult with the City to agree whether a changed condition is present or if a request for adjustment as to time and money is appropriate.

## **SECTION 3 ROLES AND RESPONSIBILITIES**

**3.1 Roles and Responsibilities Generally.** Sound Transit will be solely responsible for all aspects of the DRLE contract, including administering the engineering, design, contracting, and construction process for the Betterments and for communicating with all consultants and its Design Builder. The City will participate in decisions regarding the Betterments as provided in this Section 3 and pay all costs as prescribed in Section 4.

**3.2 City's Participation in Engineering and Design.** The City has dual roles concerning the Project and Betterments, as the land use regulator for the entire DRLE Contract work within city limits, and as the owner of the Betterments. When exercising its rights under this Agreement, the City will participate in design reviews and approvals for work related to the Betterment work. City will expedite reviews and responses to all requests, and shall not unreasonably withhold approval or responses to reasonable requests. The City will respond within 14 calendar days for major design submittals and 7 days for issued for construction submittals, unless otherwise agreed. The City may request a time extension for review. If the City fails to timely respond, Sound Transit may proceed with that specific submittal or review if it has determined that it is in the best interest of the Project. However, all project correspondence and communications with the Design Builder and its designers will be by and through appropriate Sound Transit personnel.

**3.2.1 Land Use Oversight.** When acting as the Project land use regulator, the City will review and respond to requests as agreed to in the Permitting Framework, approved as part of the Project Administration Agreement executed January 22, 2019.

**3.3 Permitting and Environmental Review.** The Parties agree that compliance with the State Environmental Policy Act (SEPA) has been satisfied for the Betterments as currently described in this Agreement. (The Betterments were included in the Downtown Redmond Link Extension 2018 SEPA Addendum to the East Link Project Final Environmental Impact Statement.) Sound Transit will include impacts to critical areas from the Betterments as currently described in this Agreement in the Environmental permit applications for the Project. Sound Transit's Design Builder will include the Betterments as necessary in other Project permit submittals. The City will rely on the SEPA compliance referenced above in issuing City permits for the Betterments and will not require additional environmental review or mitigation. The City will be responsible for any additional environmental review, permitting, or mitigation resulting from any revision to or change in scope of the Betterments as currently described in this Agreement and for any unanticipated costs beyond what Sound Transit would otherwise incur for its own Project permits as a result of including the City's Betterments.

**3.4 Procurement.** Sound Transit has included all City Betterments in its DRLE RFP. The City may participate as an advisor for the Sound Transit DRLE RFP, and will be invited to all discussions with Proposers related to the Betterments to be performed on behalf of the City. The City will be invited to participate in such meetings and negotiations that are related to the City Betterments. Sound Transit will maintain the right, at its sole discretion, to request best and final

offers, and the right to meet or negotiate with the proposers and make all procurement determinations.

**3.5 Termination of Betterments Prior to Award.** One week before the Sound Transit Board authorizes the Sound Transit Chief Executive Officer to execute the DRLE Contract, the City may, at the City's sole discretion, provide a written notice to Sound Transit requesting that Sound Transit remove any or all of the Betterments from the DRLE Contract, and from this Agreement. Sound Transit will give the City as much prior notice as possible before the Board meeting where the DRLE Contract will be acted upon. Upon such timely notice, Sound Transit agrees to withdraw the work from the scope of the DRLE Contract, and this Agreement will be amended in accordance with Section 8.

**3.6 Utility Relocation.** In the event construction of any of the Betterments requires the relocation of any utility which was not otherwise required to be relocated by the Project, the City will bear full responsibility for relocating those utilities.

**3.7 Observance of Construction Work.** The City may observe the performance of any construction work related to the Betterments, and may request through Sound Transit additional quality verification or testing of Betterments work beyond that required by a City code, standard, or permit condition, at the City's expense. The City will request access to observe the Betterments' construction work, and Sound Transit will not unreasonably deny any such requests. By exercising its right to observe or inspect the Betterments work, the City will not be deemed or construed to be in control of the Betterments.

**3.8 City to Act in Timely Manner.** The City will perform its obligations under this Agreement in a timely manner. If the City fails to perform any of its obligations within the express timeframes set forth in this Agreement or a reasonable timeframe where no express timeframe is established and such delay causes Sound Transit to incur additional costs under the DRLE Contract, the City will be liable for all costs owed to the Design Builder by Sound Transit due to the City's failure to timely perform.

## **SECTION 4 FUNDING**

**4.1 Cost Estimate.** The parties have agreed to an "Estimated Total Betterments Amount" that the City will pay to Sound Transit for the Betterments under this Agreement that is attached as **Exhibit E**. The Parties acknowledge that the Estimated Total Betterments Amount will be replaced with the bid item price in the DRLE Contract after award.

**4.2 City Pays Actual Costs.** This Estimated Total is an estimate of the amount of reimbursement of costs Sound Transit incurs for administering, procuring, designing, acquiring property and completing construction of the Betterments, including applicable taxes. The City acknowledges that it has funding available for the Estimated Total Betterments Amount, and that it will provide funding for any valid increases to this amount in accordance with this Agreement. Sound Transit agrees to notify the City in writing if it anticipates that the total reimbursement amount for all costs the City is required to pay under this Agreement will exceed the then-current Estimated Total Betterments Amount identified in **Exhibit E**, as may be amended in writing by

the Parties. Notwithstanding any estimated cost, the City agrees to pay costs arising out of or related to the Betterments, as defined in this Agreement.

**4.2.1 City Pays Fixed Administrative Costs.** The estimate for the Betterments in Exhibit E includes fixed administrative costs which will cover Sound Transit's administrative costs related to the Betterments established in Section 2.1 and any changes to the Betterments initiated by Sound Transit or the Design Builder. The administrative costs include costs for Sound Transit staff oversight and consultant fees. The City will pay 50% of the administrative costs as a lump sum within 30 days after the execution of this Agreement. The remaining 50% will be paid proportionately based on percentage of completion as described in the Design Builder's payment applications in accordance with Section 4.3.

**4.3 Invoicing and Payment Schedule.** The City will pay Sound Transit for all actual and valid costs incurred by Sound Transit for final design and construction of the Betterments, and related sales tax, pursuant to the following terms:

- (a) **Invoicing.** Sound Transit will invoice the City quarterly, and the City will pay within 30 days of receipt of an adequately supported invoice. The City will have access to all support documentation for all invoiced amounts.
- (b) **Invoice Contents.** Invoices for the Betterments will include:
  - (i) Actual payments made by Sound Transit to the Design Builder for confirmed progress on activities billed to the Betterments work;
  - (ii) Sound Transit's property acquisition costs associated with the Betterment work;
  - (iii) Sound Transit's administrative costs in accordance with Section 4.2.1;
  - (iv) Any other costs incurred as agreed herein; and
  - (v) Supporting documentation to support the invoice amount.
- (c) **RCCI Payments.** The City and Sound Transit will share equally the costs of the RCCI work as described in Exhibit D. Sound Transit will invoice the City for its share of the RCCI work performed during each invoicing period.

**4.4 Payment for Changes.** If the City requests changes to the work (design or construction) and such changes are accepted by Sound Transit pursuant to Section 2.2.1 of this Agreement, the City will pay for all additional costs associated with the changed work scope, including Sound Transit administrative costs. For City initiated change orders, Sound Transit will add an administrative fee of 16% to the total cost of the change as charged by the Design Builder. For all changes orders other than those initiated by the City, the City will pay for all direct costs associated with approved changes to the Betterments, without any additional Sound Transit administrative costs.

**4.5 Payment for Design Builder Claims.** The City will pay that portion of Sound Transit's reasonable defense costs (attorney's fees and consultant's fees, expenses, and costs;

alternative dispute resolution and court costs and expenses), and any settlements, judgments, or awards attributable to the City or to the Betterments (collectively, "Actual Costs") arising out of claims or disputes the Design Builder asserts against Sound Transit related to or arising from the Betterments, provided that Sound Transit includes the City in the claims process and the City approves the settlement; however, the City will not be liable to pay any portion of claims which arise due to the sole negligent acts or omissions of Sound Transit. If such claims result in legal action, including, but not limited to, any alternative dispute resolution processes, Sound Transit will defend, in coordination with the City, against such claims. To the extent such claims were not caused by the negligent acts or omissions of Sound Transit or its Design Builder, the City will pay Sound Transit for its Actual Costs to the extent that the claims or disputes arise from or relate to the Betterments. To the extent that such claims were the result of the concurrent negligence of the Parties, the Parties shall be responsible only to the extent of the Party's own negligence. For claims that relate to the RCCI, the Parties will share the Actual Costs equally.

Sound Transit must demonstrate that a claim arises from or relates at least in part to the Betterments before the City is obligated to pay. In the event claims include both Betterment and other DRLE Contract scope, the City will be liable only for the Betterments' contributions to Sound Transit's Actual Costs as determined by the final value of the claims.

**4.6 Late Payments.** If the City fails to pay any undisputed invoice amount due hereunder for 30 days after receiving a past due notice from Sound Transit, and if such late payment is not excused by Sound Transit, then the City will pay interest on such undisputed past due amount at an interest rate of 1 percent per month commencing 10 days after the date such undisputed amount is due, until paid. Payment of such default interest will not excuse or cure any default by the City under this Agreement and will not affect Sound Transit's suspension rights under Section 8.1 of this Agreement.

## **SECTION 5 ACCEPTANCE OF WORK, TRANSFER OF TITLE, & WARRANTIES**

**5.1 Acceptance of Work, Transfer of Title, & Warranties.** Following reasonable notice by Sound Transit, the City will attend acceptance walk-through inspections and participate in creating punch-lists for the Betterments, consistent with the terms of the DRLE Contract. If the City fails to attend such inspections, the City waives its right to inspect the Betterments except to the extent such inspections are required by City codes, standards or permits. When all punch-list items are addressed or corrected to the DRLE Contract requirements or the City's satisfaction, the City will issue Sound Transit a letter of acceptance giving final approval for each Betterment. Upon acceptance of the Betterments by the City and Sound Transit, Sound Transit will accept the Betterment from the Design Builder and then convey any and all interest of Sound Transit in the Betterments to the City by an appropriate bill of sale or other conveyance instrument. Sound Transit will also assign any and all warranties relating to the Betterments to the City in order to enable the City to address directly with the Design Builder any defects in workmanship or materials. Acceptance of the Betterments shall occur at upon completion of the Project unless the Parties agree that earlier acceptance would benefit the Parties. As a condition of early acceptance, the City may require that the Design Builder remain responsible for and repair any damage to the Betterment occurring while the Betterment area is under traffic control or similarly under the



Design Builder's possession and control. Sound Transit makes no warranties, express or implied, for the Betterments, beyond those set forth in Section 17.2 of this Agreement.

## **SECTION 6 MAINTENANCE AND OPERATION**

**6.1 All Betterments.** The City will be responsible for all maintenance, repair, and operation of the Betterments upon transfer of title as contemplated in paragraph 5.1 herein, unless the Parties mutually agree in writing that Sound Transit will perform certain maintenance activities. All operation and maintenance will meet City standards and also be consistent with Sound Transit maintenance standards for that Betterment that is located on Sound Transit property and with WSDOT requirements for structures within WSDOT right-of-way.

## **SECTION 7 PROPERTY RIGHTS**

**7.1 Property Map.** Attached to this Agreement as Exhibit F is a drawing showing the various property rights necessary for the construction and operation of the Betterments as referred to in this Section 7.

**7.2 PRV.** The PRV will be constructed within rights-of-way owned either by WSDOT or the City. The City will grant the Design Builder all rights necessary on City property for the construction of the PRV. If the Parties agree that additional property is needed beyond that owned by WSDOT and the City to complete construction of the PRV, Sound Transit agrees to acquire all real property rights necessary for construction and the City will reimburse Sound Transit for the actual costs of acquiring the necessary property. The City will be responsible for acquiring any additional real property necessary for the operation and maintenance of the PRV.

**7.3 70th Street.** The City agrees to acquire and convey to Sound Transit all real property rights necessary for construction of 70th Street in accord with the following:

**7.3.1** The City will acquire possession and use of all property identified by the Parties as necessary for 70th Street ("70th Street Property"), as shown in Exhibit F, and convey the necessary access rights to Sound Transit no later than December 31, 2020. If the City has not acquired possession and use of the 70th Street Property by December 31, 2020, the City will be liable to the Sound Transit for all costs incurred due to the delay. If the City is unable to deliver possession and use of the 70th Street Property to Sound Transit by January 31, 2022, the 70th Street scope of work will be terminated pursuant to Section 8.2 of this Agreement.

**7.3.2** If the Parties agree that additional property is needed beyond that already owned or acquired by the City in accordance with Section 7.3.1 to complete construction of 70th Street, Sound Transit agrees to acquire all real property rights necessary for construction and the City will reimburse Sound Transit for the actual costs of acquiring the necessary property.

**7.4 76th Street.** The Parties have concluded, based on preliminary design, that no additional property rights are needed to construct the 76th Street Betterment. If the Parties agree

that additional property is needed, Sound Transit agrees to acquire and convey to the City all real property rights necessary for construction, operation, and maintenance of 76th Street. In addition to the actual costs for the design and construction of the 76th Street, the City will reimburse Sound Transit for the actual costs of acquiring the necessary property. The parties agree that this sum is full compensation to Sound Transit for all property rights to be conveyed to the City.

## **7.5 RCCI.**

**7.5.1 Park mitigation.** Sound Transit agrees to acquire and convey to City the “Cleveland Street Properties”, more clearly denoted as properties RL 190, RL 191, RL 194, and RL 195. These properties or a portion of them will be encumbered with a protective covenant with King County as a replacement for impacts to other City of Redmond park property. The exact area and delineation of the covenant will be determined after the impacts are fully known. The Parties intend to execute a Letter of Concurrence to more fully document this process.

**7.5.2 RCC Reconfiguration.** Sound Transit agrees to acquire and convey to the City a portion of property on the east side of 166th for purposes of the realignment of the RCC. The City agrees to reimburse Sound Transit for half of the cost of the actual ROW purchase costs.

## **SECTION 8 ADDITIONAL BETTERMENTS**

**8.1 40th Street Underpass.** Sound Transit has also included a pedestrian underpass beneath NE 40th Street adjacent to the light rail guideway to the west of 156th Avenue in the DRLE RFP (“Underpass”). The City will participate in the Underpass in the same capacity as the Betterments. All terms and provisions of this Agreement will apply to the Underpass with the exclusion of Sections 3.3, Permitting and Environmental Review; 3.5, Termination Prior to Award; 3.6, Utility Relocation; 4, Funding; 5, Acceptance of Work, Transfer of Title, & Warranties; 6, Maintenance and Operation; 7, Property Rights; 9, Suspension and Termination; 10, Ownership and Use of Deliverables; and 12.2 Environmental Matters.

**8.2 Future Agreement Necessary.** Sound Transit anticipates it will enter into a betterment funding agreement with Microsoft for the design and construction of the Underpass. While Microsoft has agreed to fund the design and construction of the Underpass, the Parties agree that the Underpass must be owned, operated, and maintained by a public entity due to its integration with the public right-of-way and other public facilities. Because the proper ownership, operation, and maintenance will be driven by final design, the construction of the Underpass is contingent upon a future ownership, operation, and maintenance agreement. The City agrees it will participate in the negotiations of the future ownership, operation, and maintenance agreement for the Underpass and acknowledges it may own and/or have a role in maintaining the Underpass facility after construction. The Parties shall determine the owner of the Underpass prior to the commencement of construction of the Underpass. The Parties will consider addressing those matters that are the subject of the excluded sections in 8.1 above in the future ownership, operation, and maintenance agreement.

## **SECTION 9 SUSPENSION AND TERMINATION**

**9.1 Suspension for Non-Payment.** Sound Transit may suspend or terminate performance of any of the Betterments if the City fails to make timely payment of any invoice from Sound Transit. If Sound Transit has not received payment from the City within thirty (30) days following receipt of invoice, or by any later date specified in such invoice, Sound Transit may suspend performance of all or any part of the work after giving the City twenty-five (25) days' written notice of Sound Transit's intent to suspend. If the City makes payment within the twenty-five (25) day period, the work will not be suspended. If the City fails to make payment, Sound Transit may suspend the work at any time after the twenty-five (25) day period expires. Such suspension will remain in effect until payment is made in full, at which time the suspension will be lifted. The City will pay or reimburse Sound Transit for all amounts Sound Transit is obligated to pay the Design Builder as compensation for any suspension or termination of work caused by the City's non-payment, including all non-cancelable obligations. The City will also be responsible to repay any grant funding for the Betterment, if required by the terms of the grant, and to reimburse Sound Transit for costs associated with redesign of the construction package to the extent, but only to the extent, that such costs are the direct result of the removal of the Betterment as an element of the package.

**9.2 Termination for Cause.** Either Party may terminate this Agreement as to any or all Betterments, for cause in the event that the other Party fails to fulfill its material obligations under this Agreement in a timely manner or breaches any material provision of this Agreement. The Party wishing to terminate this Agreement for cause will provide the other Party with notice of its intent to terminate and will give the other Party an opportunity to correct the failure to perform or breach within thirty (30) days of the notice. If the failure or breach is not corrected or cured, this Agreement may be terminated by the aggrieved Party by giving seven (7) days' written notice to the other Party. If Sound Transit terminates this Agreement for the City's failure or breach, the City will reimburse Sound Transit for all Betterments work satisfactorily completed up to the date of termination and for all construction contract closing costs, and will reimburse Sound Transit for any costs incurred by Sound Transit for redesign of the construction package to the extent, but only to the extent, that such costs are the direct result of the removal of the Betterments as an element of the package.

**9.3 Termination by City for Material Alteration.** The City may also terminate this Agreement as to any of the Betterments in the event the City determines that the specifications for the Betterment described in the documents produced by the Design Builder, while remaining within the DRLE Contract requirements, materially alter the functionality, design/appearance, or ease or costs of maintenance of the Betterment. Upon termination under this Subsection 8.3, the City will pay for all services provided by the Design Builder prior to termination and will pay all costs incurred by Sound Transit associated with the termination of the Betterment scope of work. Sound Transit will, if the City so requests, (i) assign to the City any and all intellectual property rights that Sound Transit owns specifically relating to the Betterments engineering and design work, and (ii) permit the City to pursue design and construction of the Betterment without Sound Transit's participation. If the City terminates its participation in any Betterment under this Subsection, Sound Transit's participation will also terminate.

**9.4 Termination by City for Cost Reasons.** The City may terminate this Agreement as to any Betterment if the Design Builder encounters a Betterment site condition that will result in construction of that Betterment's scope of work significantly exceeding the bid price amount and a means to reduce the cost through modification of design cannot be satisfactorily achieved, then the City may terminate that Betterment that will significantly exceed the contract amount. The City will pay all costs incurred by Sound Transit associated with the termination of the Betterment scope of work.

**9.5 Termination by Sound Transit for Cost Reasons.** Sound Transit may terminate this Agreement as to any Betterment if the Design Builder encounters a Betterment site condition that will result in construction of any of the Project scope of work significantly exceeding the contract amount and a means to reduce the cost through modification of design cannot be satisfactorily achieved, then Sound Transit may terminate that Betterment that will cause Project scope to significantly exceed the contract amount.

**9.6 Additional Termination Rights of Sound Transit.** Sound Transit may terminate this Agreement if, in the reasonable determination of Sound Transit, any Betterment design or construction would prevent or significantly impair Sound Transit's ability to complete construction of the Project within the project schedule. If any granting agency requires repayment of the grant funding due to Sound Transit's termination under this Section 8.5, Sound Transit will repay the grant funds. Sound Transit will also refund any monies paid by the City for easements or other property rights in the Sound Transit Property that will not be utilized as the result of the termination or convey such property rights to the City.

**9.7 Procedure Prior to Termination.** Prior to Sound Transit termination this Agreement as provided for in Section 9.5 or Section 9.6, Sound Transit shall first notify the City of its intent to terminate and engage the City and Design Builder in good faith efforts to develop a solution that avoids termination. Consistent with Section 1.2 of this Agreement, Sound Transit's notice of intent to terminate shall be as early as possible in the process, and the Parties shall work expeditiously to determine whether a mutually agreeable solution exists. If Sound Transit decides to terminate after such a process, the termination procedures of Section 9.8 apply unchanged.

**9.8 Termination by Mutual Agreement.** This Agreement will also terminate with the mutual consent of both parties.

**9.9 Procedure upon Termination.** If this Agreement is terminated for any of the reasons set forth in above, the Parties will proceed as follows:

**9.9.1** Sound Transit will assign to the City any and all intellectual rights that Sound Transit owns specifically related to the Betterments engineering and design work in order to enable the City to proceed with completion of design and construction of the Betterments if the City so desires. The assignment will disclaim all express and implied warranties by Sound Transit including but not limited to, the implied warranties or merchantability, fitness for a particular purpose, and sufficiency and completeness of design.

**9.9.2** Sound Transit will be reimbursed for any costs or charges incurred by Sound Transit design, engineering, and construction work satisfactorily completed prior to such termination unless otherwise specified in this Agreement.

**9.9.3** The invoicing provisions in Section 4.3 will continue to apply for all costs incurred related to the Betterments prior to termination.

**9.10 No Release of Liability.** Except as provided in this Section 9, a termination by either Party will not extinguish or release either Party from liability, claims, or indemnity obligations existing as of the date of termination, including consultant and contractor claims and costs incurred by said Party in relation to the Betterments work and any requirement contained in the terms of any grant for repayment of the grant funds. Any costs incurred prior to proper notification of termination will be borne by the Parties in accord with the terms of this Agreement. The Parties agree to work together cooperatively to develop a coordinated plan for termination, including the determination of reasonable redesign and costs associated with the Project work.

## **SECTION 10 OWNERSHIP AND USE OF DELIVERABLES**

Sound Transit will assign its rights to the engineering and design work related to each Betterment prepared and developed under this Agreement to the City upon acceptance of that Betterment by the City and payment by the City in full of all amounts owed under this Agreement for that Betterment, subject to the obligation of Sound Transit to assign its intellectual property rights in that work to the City in the event that this Agreement is terminated as provided in Section 8. This assignment will disclaim all warranties by Sound Transit including but not limited to, the implied warranties of merchantability, fitness for a particular purpose, and sufficiency of design.

## **SECTION 11 INSURANCE**

Sound Transit will ensure that its Design Builder, its subcontractors and consultants will maintain insurance as required by Sound Transit in the DRLE Design Build Contract. Such insurance will be primary and non-contributory and will include, but not be limited to, Commercial General Liability Insurance, Commercial Automobile Liability Insurance, Workers Compensation, Builders Risk, and Professional Liability insurance. Sound Transit will require the City be named as an additional insured for all work and operations associated with the Betterments on any Commercial General Liability Insurance, Commercial Automobile Liability Insurance, and Builders Risk insurance. In addition, Sound Transit shall require its Design Builder and subcontractors to indemnify and hold harmless the City in the same manner and to the same extent as Sound Transit, including the waiver of any industrial insurance immunity, for the Betterment work.

## **SECTION 12 INDEMNITY**

**12.1 General Indemnity.** Each Party to this Agreement will defend, indemnify and save harmless the other Party, its officers, officials, employees, and agents, while acting within the

scope of their employment as such, from any and all costs, claims, judgments, and/or awards of damages (both to persons and property), arising out of, or in any way resulting from, each Party's negligent acts or omissions. No Party will be required to defend, indemnify or save harmless the other Party if the claim, suit, or action for injuries, death, or damages (both to persons and property) is caused by the sole negligence of the other Party. Where such claims, suits, or actions result from concurrent negligence of the Parties, or involve those actions covered by RCW 4.24.115, the indemnity provisions provided herein will be valid and enforceable only to the extent of the Party's own negligence. Each Party agrees that its obligations under this subparagraph extend to any claim, demand, and/or cause of action brought by, or on behalf of, any of its employees or agents. **For this purpose, each Party, by mutual negotiation, hereby waives, with respect to the other party only, any immunity that would otherwise be available against such claims under the Industrial Insurance provisions of Title 51 RCW.**

**12.1.1 City Inspection Personnel.** The City's obligation to indemnify under this paragraph does not extend to claims, demands, and/or causes of action brought by, or on behalf of, City inspection personnel for injuries arising while such inspection personnel are engaged in construction inspection in the normal course of the City's permitting and code compliance process as a regulatory agency. The City acknowledges that the City inspections under this Agreement are not for permitting and code compliance purposes and are undertaken by the City as a participant in the development of the Betterments. The Parties acknowledge that the City's inspections for permitting and code compliance purposes are outside the scope of this Agreement. The City shall not be required to indemnify Sound Transit from claims for injuries to City inspection personnel while engaged in permitting and code compliance inspection and the City's waiver of its immunity under Title 51 RCW shall not apply to such claims.

**12.2 Environmental Matters.** Sound Transit will be responsible for any fines or penalties, and will indemnify the City for reasonable related costs, caused by failure by Sound Transit to obtain or comply with required environmental permits, and for consequences and any required remediation resulting from Sound Transit's failure to handle hazardous substances or waste with due care during the performance of the City Betterment scopes of work. The Parties will each have the right to participate in settlement or abatement discussions related to environmental citations or enforcement actions.

The City will indemnify Sound Transit, its agents, the Design Builder, and employees for any environmental liability of any type, related to existing known or unknown conditions on the site of the Betterments, whether on the City right of way or other property. To the extent allowed by law, the City agrees to release Sound Transit and its agents, employees, and contractors from any continuing obligations, responsibility or liability related to known or unknown environmental conditions that were existing at the time of work performed under this Agreement, provided that this liability is not the result of Sound Transit or the Design Builder's failure to handle new or pre-existing hazardous substances with due care and provided that the negligent actions of Sound Transit, the Design Builder, or their agents, employees, and contractors, do not exacerbate any such pre-existing environmental conditions. To the extent allowed by law, the City will reimburse Sound Transit for the cost of any environmental investigations, disposal, or cleanup activities conducted by Sound Transit to the extent the need for said investigation resulted from hazardous

substances for which the City is responsible. The City will provide Sound Transit with notices of any inspections related to the Betterments during the term of this Agreement.

**12.4 Survival.** The indemnification obligations provided in this Section 11 will survive termination of this Agreement.

## **SECTION 13 DISPUTE RESOLUTION**

**13.1 Exclusivity.** Neither Party will take or join any action in any judicial or administrative forum to challenge actions of the other Party associated with this Agreement or the Betterments, except as set forth herein.

**13.2 Scope-Cooperation.** Any disputes or questions of interpretation of this Agreement that may arise between the Parties will be governed under the dispute resolution provisions in this Section. The Parties agree that cooperation and communication are essential to resolving issues efficiently. The Parties agree to exercise their best efforts to resolve any disputes that may arise through this dispute resolution process at the lowest level possible.

**13.3 Process.** The Parties agree to use their best efforts to resolve disputes arising out of or related to this Agreement, and the Designated Representatives will use their best efforts to resolve any conflicts before initiating the dispute escalation process. If the Designated Representatives are unable to resolve the issue, the Parties will undertake good faith negotiations by engaging in the following dispute escalation process should any such disputes arise:

**13.3.1 Level One -** Sound Transit's DRLE Executive Project Director and the City's Light Rail Project Director will meet to discuss and attempt to resolve the dispute in a timely manner. If they cannot resolve the dispute within fourteen (14) days after referral of that dispute to Level One, either Party may refer the dispute to Level Two.

**13.3.2 Level Two -** Sound Transit's Executive Director of Design, Engineering and Construction Management and the City's Public Works Director will meet to discuss and attempt to resolve the dispute, in a timely manner. If they cannot resolve the dispute within fourteen (14) days after referral of that dispute to Level Two, either Party may refer the dispute to Level Three.

**13.3.3 Level Three -** Sound Transit's Chief Executive Officer or Designee and the City Mayor or Designee will meet to discuss and attempt to resolve the dispute in a timely manner.

**13.4 Legal Action.** Except as otherwise specified in this Agreement, in the event the dispute is not resolved at Level Three within twenty-one (21) days after referral of that dispute to Level Three, the Parties are free to file suit, seek any available legal remedy, or agree to alternative dispute resolution methods such as mediation or arbitration. At all times prior to resolution of the dispute, the Parties will continue to perform any undisputed obligations and make any undisputed required payments under this Agreement in the same manner and under the same terms as existed prior to the dispute. Neither Party has any obligation to agree to refer the dispute to mediation,



arbitration, or other form of dispute resolution following completion of Level Three of the process described herein. Such agreement may be withheld for any reason or no reason.

## **SECTION 14 REMEDIES AND ENFORCEMENT**

**14.1 Reservation of Rights.** The Parties reserve the right to exercise any and all of the following remedies, singly or in combination, and consistent with the dispute resolution and default Sections of this Agreement, in the event the other violates any provision of this Agreement:

**14.1.1** Commencing an action at law for monetary damages;

**14.1.2** Commencing an action for equitable or other relief related to the Betterments;

**14.1.3** Seeking specific performance of any provision that reasonably lends itself to such remedy; and/or

**14.1.4** The prevailing party (or substantially prevailing party if no one party prevails entirely) will be entitled to reasonable attorney fees and costs.

**14.2 Remedies Cumulative.** All remedies set forth above are cumulative and the exercise of one will not foreclose the exercise of others.

**14.3 No Waiver.** Neither Party will be relieved of its obligations to comply promptly with any provision of this Agreement by reason of any failure by the other Party to enforce prompt compliance, and such failure to enforce will not constitute a waiver of rights or acquiescence in the other Party's conduct.

## **SECTION 15 CITY'S PERMITTING AND REGULATORY AUTHORITY**

**15.1 No Waiver of Authority.** Nothing in this Agreement will be deemed a waiver of the City's regulatory or permitting authority as to any of the permits required for the Project, nor a predetermination of the compliance of any work with applicable codes and regulations. The City retains the right to approve, disapprove, or condition any City permits required for the Betterments and the Project within the bounds of the City's legal authority.

## **SECTION 16 DURATION OF AGREEMENT**

**16.1 Duration.** This Agreement will take effect upon the last date of signature by the Parties as set forth below. This Agreement will remain in effect until all obligations of both Parties have been performed, unless this Agreement is sooner terminated as provided in Section 8 above.

## **SECTION 17 COVENANTS AND WARRANTIES**

**17.1 The City's Warranties.** By execution of this Agreement, the City warrants:

**17.1.1** That the City has the full right and authority to enter into and perform this Agreement, and that by entering into or performing this Agreement the City is not in violation of any law, regulation or agreement by which it is bound or to which it is bound or to which it is subject; and

**17.1.2** That the execution, delivery and performance of this Agreement by the City has been duly authorized by all requisite corporate action, that the signatories for the City hereto are authorized to sign this Agreement, and that upon approval by the City, the joinder or consent of any other party, including a court or trustee or referee, is not necessary to make valid and effective the execution, delivery and performance of this Agreement.

**17.2 Sound Transit's Warranties.** By execution of this Agreement, Sound Transit warrants:

**17.2.1** That Sound Transit has the full right and authority to enter into and perform this Agreement, and that by entering into or performing this Agreement Sound Transit is not in violation of any law, regulation or agreement by which it is bound or to which it is bound or to which it is subject; and

**17.2.2** That the execution, delivery and performance of this Agreement by Sound Transit has been duly authorized by all requisite corporate action, that the signatories for Sound Transit hereto are authorized to sign this Agreement, and that upon approval by Sound Transit, the joinder or consent of any other party, including a court or trustee or referee, is not necessary to make valid and effective the execution, delivery and performance of this Agreement.

## **SECTION 18 ADMINISTRATION OF AGREEMENT**

**18.1 Joint Administration.** This Agreement will be jointly administered by Sound Transit's Designated Representative and the City's Designated Representative.

**18.2 Costs.** Each Party will bear its own costs of administering this Agreement.

## **SECTION 19 ASSIGNMENT AND BENEFICIARIES**

**19.1 Consent Required.** Neither Party may assign all or any portion of this Agreement without the express written consent of the other Party.

**19.2 Successors and Assigns.** This Agreement will be binding upon and inure to the benefit of the successors and assigns of the City and Sound Transit.

**19.3 No Third-Party Beneficiaries.** This Agreement is made and entered into for the sole protection and benefit of the Parties hereto and their successors and assigns. No other person will have any right of action based upon any provision of this Agreement.

## SECTION 20 DESIGNATED REPRESENTATIVES

**20.1 Designated Representatives.** To provide for consistent and effective communication, each Party will designate a representative (“Designated Representative”) to be responsible for coordination of communications between the Parties and to be the primary point of contact for each Party. The Designated Representatives will communicate regularly to discuss the status of the Work, identify upcoming decisions and provide any information or input necessary to inform those decisions. All notices and interagency correspondence related to this Agreement will be sent to the following Designated Representatives. The Designated Representatives are identified in **Exhibit G**. Either Party may replace its Designated Representative by providing written notice to the other Party’s Designated Representative.

**20.2 Communication.** Communication of issues, changes, or problems that may arise with any aspect of the Betterments should occur as early as possible in the process, and not wait for specific due dates or deadlines. The Designated Representatives will use reasonable efforts to provide up-to-date and best available information to the other party promptly after such information is obtained or developed.

**20.3 Communication with Design Builder.** Any correspondence or communications related to the Betterments except correspondence or communications arising from the City’s permitting authority will be made exclusively by and through Sound Transit’s Designated Representative.

## SECTION 21 NOTICES

**21.1 Designated Representatives.** Unless otherwise provided herein, all notices and communications concerning this Agreement will be in writing and addressed to the Designated Representatives.

**21.2 Delivery.** Unless otherwise provided herein, all notices will be either: (i) delivered in person, (ii) deposited postage prepaid in the certified mails of the United States, return receipt requested, (iii) delivered by a nationally recognized overnight or same-day courier service that obtains receipts, or (iv) delivered electronically to the other party’s Designated Representative as listed herein. However, notice under Section 5, Suspension and Termination, must be delivered in person or by certified mail, return receipt requested.

## SECTION 22 AUDITS

**22.1 Records to be Maintained.** Sound Transit will maintain accounts and records, including contract and financial records, which sufficiently and properly reflect all direct and indirect costs of any nature expended for work performed under this Agreement so as to ensure proper accounting for all monies paid by the City to Sound Transit. These records will be maintained for a period of six (6) years after termination or expiration of this Agreement unless permission to destroy the records is granted by the City and the Office of the Archivist pursuant to RCW Chapter 40.14. The City will have the right to inspect, review, and audit Sound Transit’s records on the Betterments at all reasonable times during regular business hours.

## **SECTION 23 GENERAL PROVISIONS**

**23.1 Governing Law and Venue.** This Agreement will be interpreted, construed and enforced in accordance with the laws of the State of Washington. Venue for any action under this Agreement will be King County, Washington.

**23.3 Time.** Time is of the essence in every provision of this Agreement. Unless otherwise set forth in this Agreement, the reference to “days” will mean calendar days. If any time for action occurs on a weekend or holiday observed by either Party, then the time period will be extended automatically to the next business day.

**23.4 No Joint Venture.** No joint venture or partnership is formed as a result of this Agreement. No employees, agents or subcontractors of one party will be deemed, or represent themselves to be, employees of any other party.

**23.5 Construction.** This Agreement has been reviewed and revised by legal counsel for all parties and no presumption or rule that ambiguity will be construed against the party drafting the document will apply to the interpretation or enforcement of this Agreement. The Parties intend this Agreement to be interpreted to the full extent authorized by applicable law.

**23.6 Costs.** Each Party will be responsible for its own costs, including legal fees, incurred in negotiating or finalizing this Agreement, unless otherwise agreed in writing by the Parties.

**23.7 Force Majeure.** The Parties will not be deemed in default with provisions of this Agreement where performance was rendered impossible by war or riots, civil disturbances, floods or other natural catastrophes beyond its control; the unforeseeable unavailability of labor or materials; or labor stoppages or slow-downs, or power outages exceeding back-up power supplies. This Agreement will not be revoked or a party penalized for such noncompliance, provided that such party takes immediate and diligent steps to bring itself back into compliance and to comply as soon as practicable under the circumstances without unduly endangering the health, safety, and integrity of both parties’ employees or property, or the health, safety, and integrity of the public, public right-of-way, public property, or private property.

**23.8 Amendments.** This Agreement may be amended only by a written instrument executed by each of the Parties hereto. The Designated Representatives may agree upon amendments to the design for the Betterments and such amendments will be binding upon the parties without the need for formal approval by the Sound Transit Board and the Redmond City Council as long as the amendments do not materially alter the functionality or design of the Betterments, do not cause the cost of the Betterments to exceed those set forth in Exhibit E, or materially increase the maintenance costs of the Betterments.

**23.9 Entire Agreement.** This Agreement constitutes the entire agreement of the Parties with respect to the subject matters of this Agreement, and supersedes any and all prior negotiations

(oral and written), understandings and agreements with respect hereto. However, the Parties will negotiate and execute such ancillary agreements as may be required to implement this Agreement.

**23.10 Headings.** Section headings are intended as information only, and will not be construed with the substance of the section they caption.

**23.11 Grammar.** In construction of this Agreement, words used in the singular will include the plural and the plural the singular, and "or" is used in the inclusive sense, in all cases where such meanings would be appropriate.

**23.12 Exhibits.** All exhibits attached to this Agreement are hereby incorporated into this Agreement.

**23.13 Counterparts.** This Agreement may be executed in several counterparts, each of which will be deemed an original, and all counterparts together will constitute but one and the same instrument.

## SECTION 24 SEVERABILITY

**24.1 Severability.** If any of the terms and conditions of this Agreement are determined to be invalid or unenforceable by a court of competent jurisdiction, the remaining terms and conditions thereby will remain in full force and effect. The Parties agree to negotiate in good faith to reform the Agreement to replace any invalid or unenforceable term and/or condition with a valid and enforceable term and/or condition that comes as close as possible to the intention of the stricken term or condition.

### CENTRAL PUGET SOUND REGIONAL TRANSIT AUTHORITY (SOUND TRANSIT)

By:   
Peter M. Rogoff, Chief Executive Officer

Date: 10/18/19

Authorized by Motion No. M2019-13

Approved as to form:

By:   
Mattelyn L. Tharpe, Legal Counsel

By:   
John Marchione, Mayor

Date: 8/8/19

Authorized by City Council  
Motion on 711619

Approved as to form:

By:   
James E. Haney, City Attorney

**EXHIBITS:**

- Exhibit A: NE 51st St. PRV and Water Main Scope and Conceptual Design
- Exhibit B: NE 70th and SR202 Scope and Conceptual Design
- Exhibit C: NE 76th St. and SR202 Scope and Conceptual Design
- Exhibit D: Redmond Central Connector Scope and Conceptual Design
- Exhibit E: Cost Estimate
- Exhibit F: Property Rights Map
- Exhibit G: Designated Representatives



## EXHIBIT A

February 27<sup>th</sup>, 2019

Kristi Wilson  
Interim Director of Public Works  
City of Redmond  
15670 NE 85<sup>th</sup> Street  
Redmond, WA, 98073

**Downtown Redmond Link Extension, Letter of Concurrence**  
**LOC 16 DRLE: NE 51<sup>st</sup> Street Utility Betterments & CFD Project No. 2 Interface**

### Purpose

The purpose of this letter is to seek concurrence from the City of Redmond for the NE 51<sup>st</sup> Street utility betterments and the interface between the Downtown Redmond Link Extension project and the City of Redmond's CFD Project No. 2 – NE 51<sup>st</sup> Street Improvements project.

### NE 51st Street Utility Betterments

The Downtown Redmond Link Extension project will add an additional 175 feet of 12" water main along with required appurtenances (tees, valves, etc.) to support the installation of a new 8" PRV station and vault for the City of Redmond. The new 8" PRV station will replace the existing PRV station located along the northern side of NE 51st St., approximately 210 feet east of the NE 51st St./SR 520 on/off-ramp intersection. This work will also include the installation of a new fire hydrant and lateral to help in set up and testing of the new 8" PRV station. The contractor shall install the PRV vault top elevation to match the grade of the sidewalk and install a gravity drain to the swale north of the PRV. Last, the DRLE Contractor will remove the existing PRV station and vault, then dispose of them as directed by the City of Redmond; this work will also include filling the existing 8" water main with CDF for abandonment by City of Redmond standards. The quantities for this betterment work are listed below:

- 175' 12" main
- 110' 8" main for PRV
- 25' 6" main for FH
- 1 8" PRV and vault per standard details 708, 709A, 709B, 710A, and 710B
- 1 FH
- 2 8" Gate Valves
- 2 12" Gate Valves
- 1 6" Gate Valve
- 2 cy yds CDF for 8" pipe abandonment
- Removal and dispose of existing 8" PRV

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*Redmond Mayor*

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*University Place Mayor*

**Paul Roberts**  
*Everett Councilmember*

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*Auburn Mayor*

**David Baker**  
*Kenmore Mayor*

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*King County Council Vice Chair*

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*King County Executive*

**Bruce Dammeler**  
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*King County Councilmember*

**Victoria Woodards**  
*Tacoma Mayor*

#### CHIEF EXECUTIVE OFFICER

**Peter M. Rogoff**



Kristi Wilson  
February 27<sup>th</sup>, 2019  
Page 2

## CFD Project No. 2 – NE 51st Street Improvements

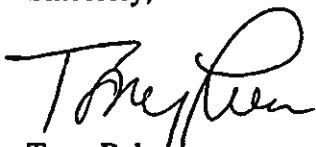
The City of Redmond is in the design phase of the CFD Project No. 2 – NE 51<sup>st</sup> Street Improvements project which improves the roadway, sidewalks, curb radii, curb ramps, traffic signal, illumination, utilities, and drainage along NE 51<sup>st</sup> Street at and near the intersection of NE 51<sup>st</sup> Street and the SR 520 on/off-ramps. The Design-Builder will complete the work shown in the Downtown Redmond Link Extension drawings with concurrence from the City of Redmond. See Attachment 1 for the description of work to be done by Sound Transit and the City of Redmond. The City of Redmond is responsible for completion of their street improvement project up to the interchange ramp terminals. The City of Redmond expects to go to construction in summer 2019, with an anticipated completion of fall 2020.

To facilitate completion of the portion of the City's project at the interface with Sound Transit's project prior to Sound Transit's work beginning at the interface, the City agrees to the following:

- Work in good faith to advertise their project for bid by April 2019.
- Include provisions in their 51<sup>st</sup> St. construction contract, such as milestones, liquidated damages, or other constraints, that will facilitate substantial completion of work at the interface by July 2020.
- Each month following NTP of the City's CFD Project No. 2, the City will assess the likeliness of substantially completing the work at the Sound Transit interface by July 2020 and take corrective action, if necessary, to ensure the milestone is met.
- In the event the City's project work at the interface is not completed by July 2020 or when Sound Transit's contractor desires to begin work, whichever is later, the City will work with ST to identify options to minimize cost or other impacts to Sound Transit's project.

The reconstruction of the interchange ramp terminal intersection will be completed by Sound Transit using the design concept provided by the City's 90% design submittal of 51<sup>st</sup>. Should you have any questions, please don't hesitate to contact me at (206) 398-5171. Please sign below and return one of the enclosed originals – please keep the additional copy for your records.

Sincerely,



Tony Raben  
Executive Project Director, DRLE

### Attachments:

1. NE 51<sup>st</sup> Street Improvements

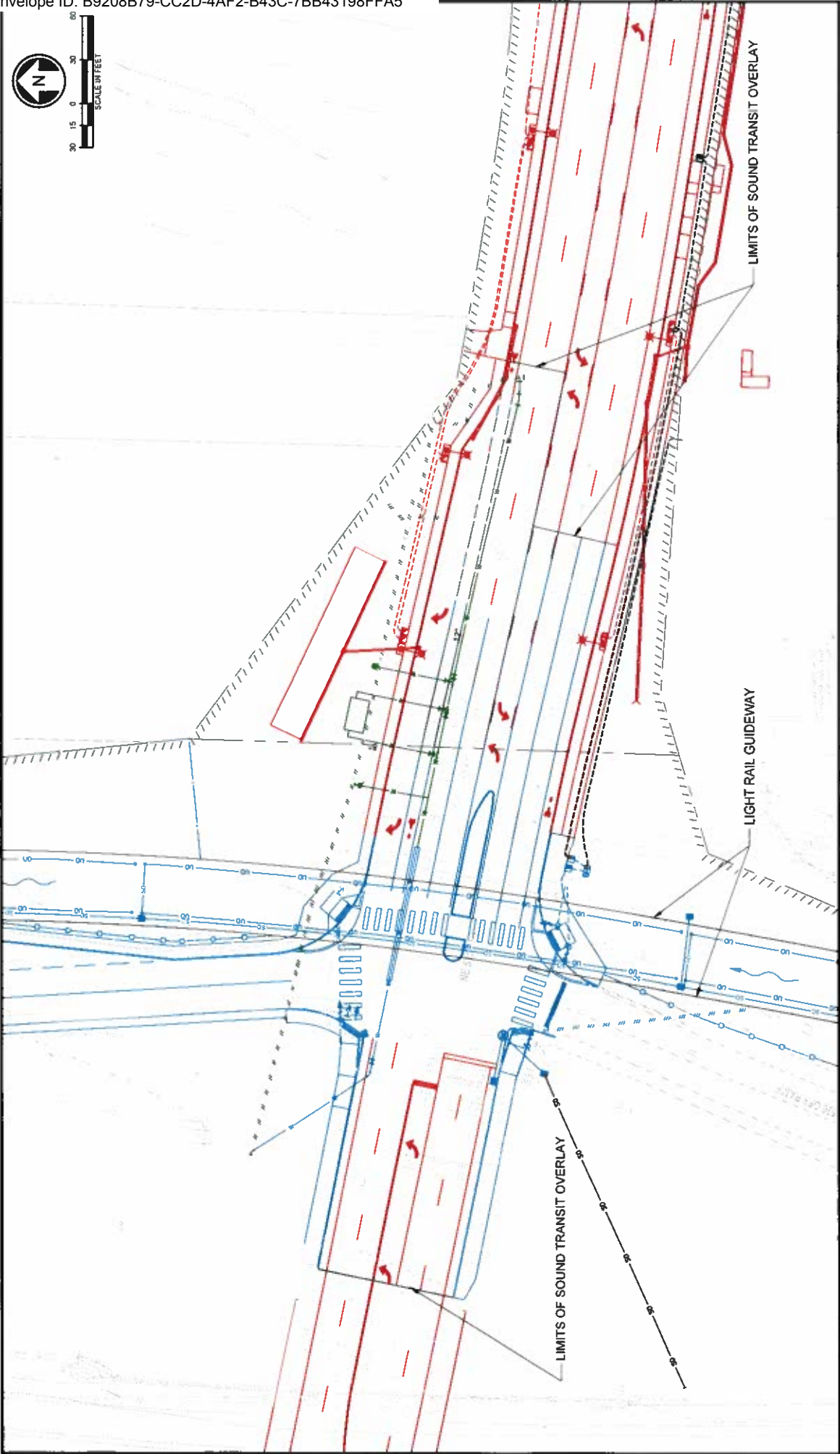
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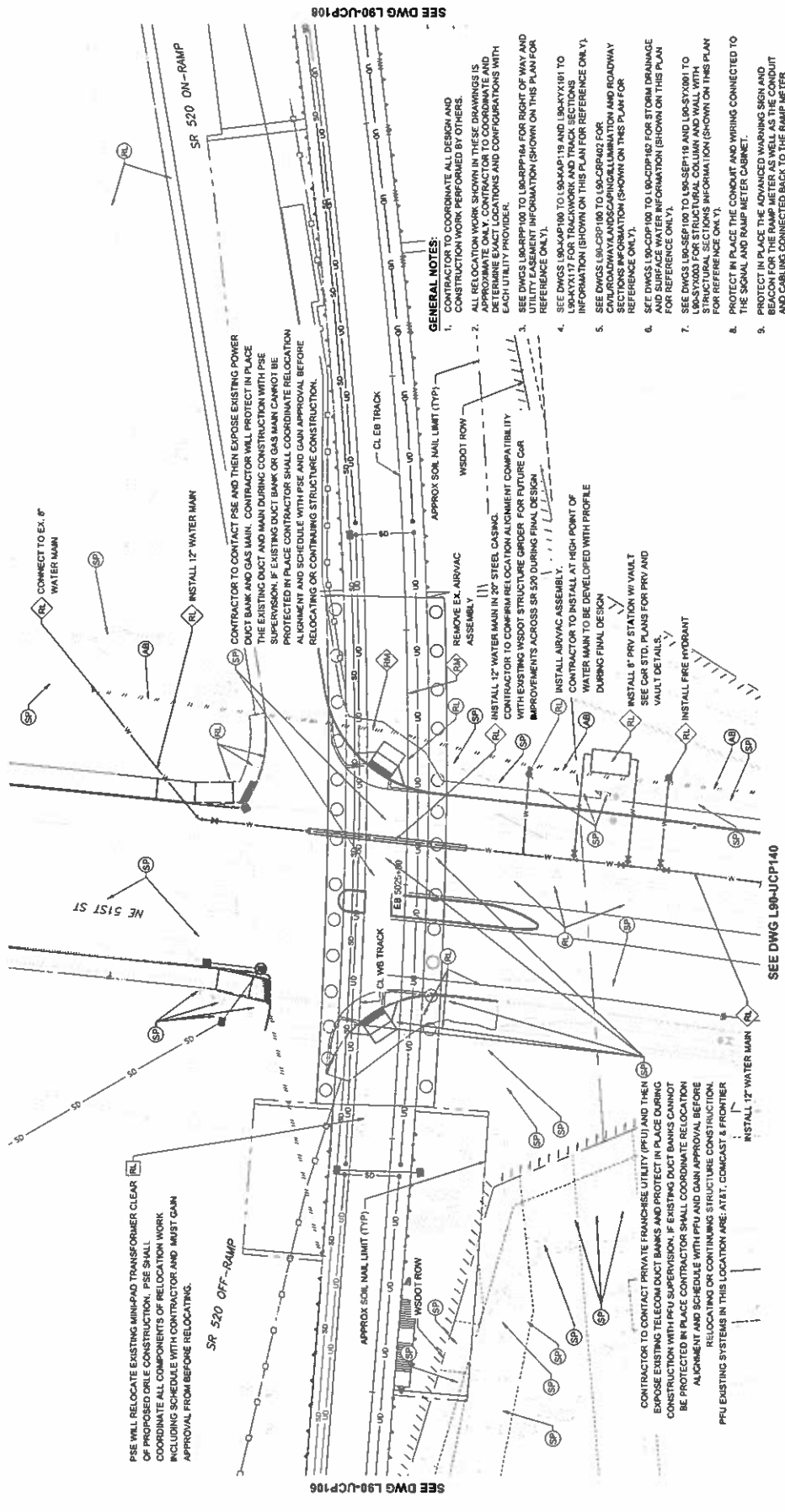
Kristi Wilson , 3/1/19  
Date

Kristi Wilson, Interim Director of Public Works  
City of Redmond

### Enclosure (s):

cc: ST Document Control





**EXISTING UTILITY INFORMATION**  
1. THE LOCATION OF ALL EXISTING UTILITIES ARE APPROXIMATE AND WERE COMPILED FROM AVAILABLE INFORMATION PROVIDED BY PUBLIC AND PRIVATE UTILITY PROVIDERS. SOME FIELD SURVEY DATA AND SITE OBSERVATIONS. THROUGH THIS COMPILATION REPRESENTS THE AVAILABLE INFORMATION OF EXISTING UTILITIES. THE INFORMATION PROVIDED HEREON MAY VARY FROM ACTUAL SIZES AND LOCATIONS.  
2. THE CONTRACTOR SHALL CONDUCT THEIR OWN RESEARCH AND INVESTIGATIONS WHICH IN THEIR OPINION ARE NECESSARY TO DETERMINE THE EXISTENCE, LOCATION, DEPTH, AND CHARACTER OF EXISTING UTILITIES AND SURFACE PENETRATIONS PERFORMED SO AS TO PREVENT THE IMPACT TO UTILITY SYSTEMS AND THEIR CUSTOMERS. SUCH WORK SHALL BE PERFORMED IN A MANNER CONSISTENT WITH THE ORDINARY STANDARD OF CARE.

**GENERAL NOTES:**  
1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.  
2. ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS BASED ON THE CONTRACTOR'S FIELD SURVEY AND DETERMINE EXACT LOCATIONS AND COORDINATIONS WITH EACH UTILITY PROVIDER.  
3. SEE DWGS L90-UCP100 TO L90-UCP140 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).  
4. SEE DWGS L90-UCP100 TO L90-UCP119 AND L90-UCP120 TO L90-UCP117 FOR TRACKWORK AND TRACK SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).  
5. SEE DWGS L90-UCP100 TO L90-UCP140 FOR CIVIL/ROADWAY/SCAFFOLDING/ILLUMINATION AND ROADWAY INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).  
6. SEE DWGS L90-UCP100 TO L90-UCP140 FOR STORM DRAINAGE INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).  
7. SEE DWGS L90-UCP100 TO L90-UCP119 AND L90-UCP120 TO L90-UCP140 FOR STRUCTURAL COLUMN AND WALL WITH STRUCTURAL SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).  
8. PROTECT IN PLACE THE CONDUIT AND WIRING CONNECTED TO THE SIGNAL AND RAMP METER CABINET.  
9. PROTECT IN PLACE THE ADVANCED WARNING SIGN AND BEACON FOR THE RAMP METER AS WELL AS THE CONDUIT AND CABLEING CONNECTED BACK TO THE RAMP METER CABINET.

**LEGEND:**  
RM = REMOVE  
AB = ABANDON IN PLACE (DESIGN-BUILD CONTRACTOR TO VERIFY IF ABANDONMENT IN PLACE IS ACCEPTABLE TO UTILITY OR CITY OF REDMOND)  
SP = SUPPORT/PROTECT DURING CONSTRUCTION (MEANS AND METHODS TO BE DETERMINED BY THE DESIGN-BUILD CONTRACTOR AND UTILITY OWNER)  
AD = ADJUST TO NEW GRADE

**SCALE:**  
1" = 20'  
SCALE IN FEET  
20 10 0 20 40

**NOT FOR CONSTRUCTION**

<b>CONCEPTUAL DESIGN</b>		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>CONFORMED SET</b>		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>DESIGNED BY</b> M. AUSTIN		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>CHECKED BY</b> J. TYLER		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>APPROVED BY</b> D. PROCTOR		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>DATE</b> 09/24/2018		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>CONTRACTOR</b> Parametrix		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>PROJECT</b> DOWNTOWN REDMOND LINK EXTENSION		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>LOCATION</b> RTS TO DOWNTOWN REDMOND		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>PRESCRIPTIVE/REFERENCE</b> COMPOSITE UTILITIES PLAN		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	
<b>EB STA 5023+00 TO EB STA 5028+00</b>		<b>DATE</b> 09/24/2018		<b>DATE</b> 03/29/2019	

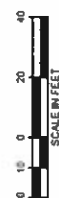
1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
2. ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS APPROXIMATE ONLY. CONTRACTOR TO COORDINATE AND OBTAIN ALL NECESSARY PERMITS, NOTATIONS AND COMPENSATIONS WITH RESPECT TO ALL RELOCATIONS.
3. SEE DWGS. 19A-SEP1040 TO 19A-SEP1046 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
4. SEE DWGS. 19A-SEP1040 TO 19A-SEP1119 AND 19A-KYX101 TO 19A-KYX1147 FOR TRACKWORK AND TRACK SECTIONS.
5. SEE DWGS. 19A-SEP100 TO 19A-SEP1402 FOR TRACKWORK, TRACKWORK EASEMENT, ILLUMINATION AND ROADWAY SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
6. SEE DWGS. 19A-SEP100 TO 19A-SEP1042 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
7. SEE DWGS. 19A-SEP100 TO 19A-SEP1119 AND 19A-SY02001 TO 19A-SY02040 FOR STRUCTURAL COLUMN AND WALL WITH REINFORCEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).



THE LOCATION OF THE EXISTING UTILITIES ARE APPROXIMATE AND WERE COMPILED FROM AVAILABLE INFORMATION PROVIDED BY PUBLIC AND PRIVATE UTILITY PROVIDERS, SOME FIELD SURVEY DATA, AND SITE OBSERVATIONS, THOUGH THIS COMPILATION REPRESENTS THE AVAILABLE INFORMATION OF EXISTING UTILITIES. THE INFORMATION PROVIDED HEREON MAY VARY FROM ACTUAL SIZES AND LOCATIONS. THE CONTRACTOR SHALL CONDUCT THEIR OWN RESEARCH AND INVESTIGATIONS WHICH IN THEIR PROFESSIONAL OPINION ARE NECESSARY TO EVALUATE THE ACCURACY OF THIS INFORMATION IN ADVANCE OF ANY CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE IMPACT OF UTILITIES AND THEIR CUSTOMERS, WHICH WORK SHALL BE PERFORMED IN A MANNER CONSISTENT WITH THE PRIMARY BOARD OF CARE.

RL = RELOCATE EXISTING UTILITY OR INSTALL NEW  
SP = SUPPORT/PROTECT DURING CONSTRUCTION  
(MEANS AND METHODS TO BE DETERMINED BY THE  
CONTRACTOR AND UTILITY OWNER)  
AG = ADJUST TO NEW GRADE

RM = REMOVE  
AB = ABANDON  
(DESIGN-BUILD)  
PLACE IS ACCE

[illegible]

## EXHIBIT B



February 5<sup>th</sup>, 2019

Kristi Wilson  
Interim Director of Public Works  
City of Redmond  
15670 NE 85<sup>th</sup> Street  
Redmond, WA, 98073

**Downtown Redmond Link Extension, Letter of Concurrence**  
**LOC 18 DRLE: Redmond Way & NE 76<sup>th</sup> Street Roadway Improvements**

### Purpose

The purpose of this letter is to seek concurrence from the City of Redmond for the Redmond Way and NE 76<sup>th</sup> Street roadway improvements and funding responsibilities for betterments, included in the Downtown Redmond Link Extension scope of work. The portions of roadway that are being agreed upon are: 1) Redmond Way from the driveway of Creekside Crossing to the intersection of Redmond Way and NE 76<sup>th</sup> Street/SR 520 WB On-Ramp, and 2) NE 76<sup>th</sup> Street from the intersection of Redmond Way to the Fred Meyer/Target traffic signal.

### Sound Transit Scope

Sound Transit will be adjusting the grade of the Redmond Way-NE 76th St./SR 520 WB On-Ramp intersection as part of light rail project to accommodate the light rail undercrossing. This scope of work includes paving, striping, adjustment of utilities to grade, retaining walls, signal work, and any additional work associated with completing the grade adjustment. See Attachment 1 which defines the scope of work limits in blue.

### City of Redmond Scope

The City of Redmond requests that Sound Transit construct, as a betterment, an additional eastbound left turn lane from Redmond Way to NE 76<sup>th</sup> Street. This scope includes:

- Design and construction of additional eastbound left turn lane and northbound receiving/thru lane on NE 76th St from Redmond Way to the traffic signal servicing Fred Meyer and Target
- Curb, gutter, pavement and lane restriping on NE 76th St. from Redmond Way to the Fred Meyer/Target traffic signal, a total distance of about 1,100 feet, that would not be required but for the betterment
- The removal of sidewalk on the existing SR 202 bridge over Bear Creek to provide enough space for the eastbound left turn lane

#### CHAIR

**Dave Somers**  
*Snohomish County Executive*

#### VICE CHAIRS

**Ron Lucas**  
*Steilacoom Mayor*

**John Marchione**  
*Redmond Mayor*

#### BOARD MEMBERS

**Nancy Beckus**  
*Auburn Mayor*

**David Baker**  
*Kenmore Mayor*

**Claudia Balducci**  
*King County Councilmember*

**Dow Constantine**  
*King County Executive*

**Bruce Dammeier**  
*Pierce County Executive*

**Jenny Durkan**  
*Seattle Mayor*

**Dave Earling**  
*Edmonds Mayor*

**Rob Johnson**  
*Seattle Councilmember*

**Kent Keel**  
*University Place Mayor*

**Joe McDermott**  
*King County Council Chair*

**Roger Millar**  
*Washington State Secretary of Transportation*

**Paul Roberts**  
*Everett Council President/ Mayor Pro Tem*

**Dave Upthegrove**  
*King County Councilmember*

**Peter von Reichbauer**  
*King County Councilmember*

**Victoria Woodards**  
*Tacoma Mayor*

#### CHIEF EXECUTIVE OFFICER

**Peter M. Rogoff**

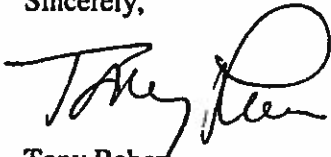
Kristi Wilson  
February 5<sup>th</sup>, 2019  
Page 2

- Revised curb taper on Redmond Way given removal of sidewalk
- Construction of new curb and sidewalk on the south side of Redmond Way near Creekside Crossing
- New channelization from the existing SR 202 bridge to the driveway of Creekside Crossing
- Any additional work required solely to complete the betterment

See Attachment 1 for improvements associated with the City of Redmond betterment described above.

Should you have any questions, please don't hesitate to contact me at (206) 398-5171. Please sign below and return one of the enclosed originals – please keep the additional copy for your records.

Sincerely,



Tony Raben  
Executive Project Director, DRLE

**Attachments:**

1. SR 202 & NE 76<sup>th</sup> Street Roadway Improvements

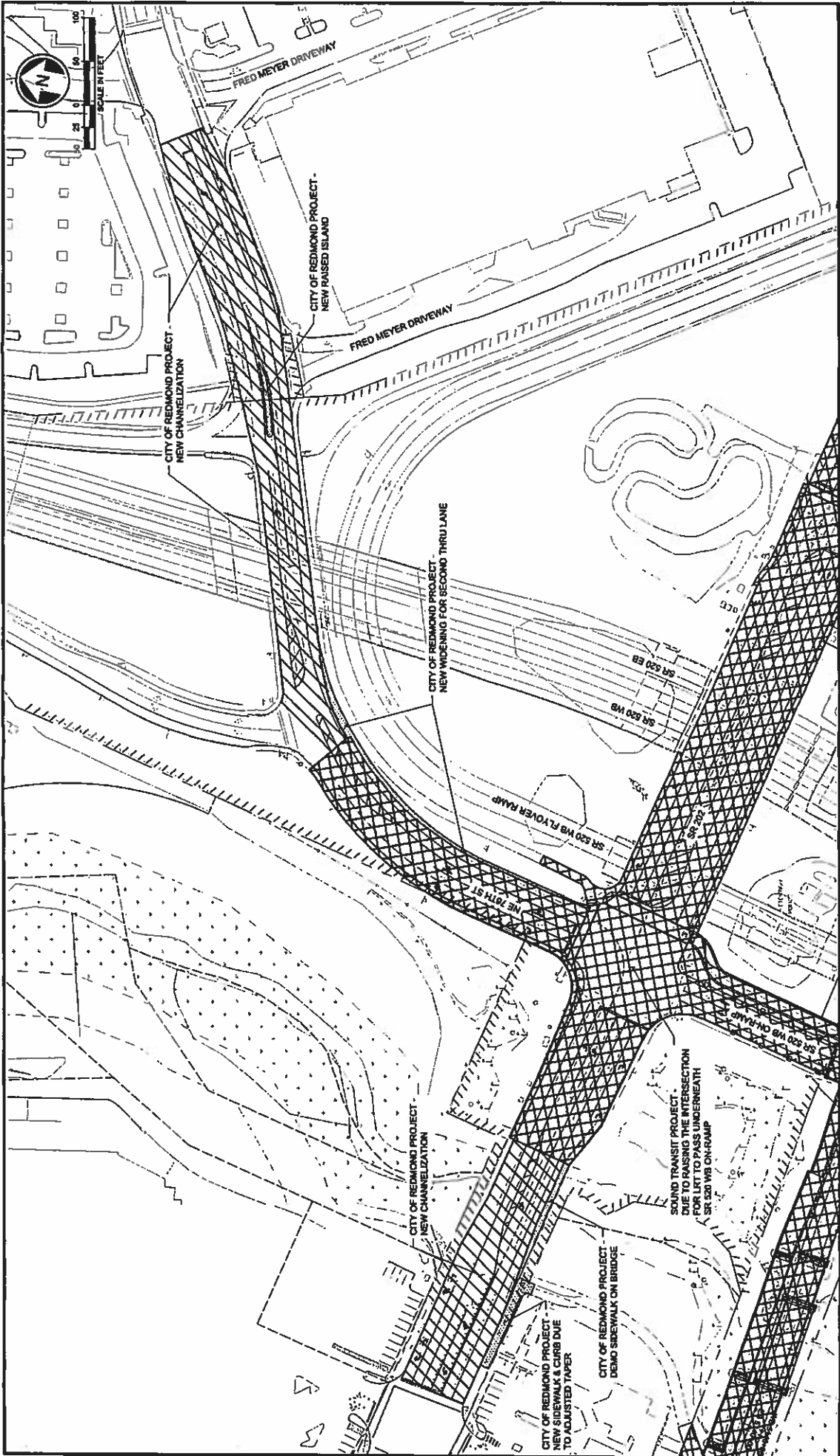
**Concurrence:**

Kristi Wilson , 2/19/19  
Date

Kristi Wilson, Interm Director of Public Works  
City of Redmond

**Enclosure (s):**

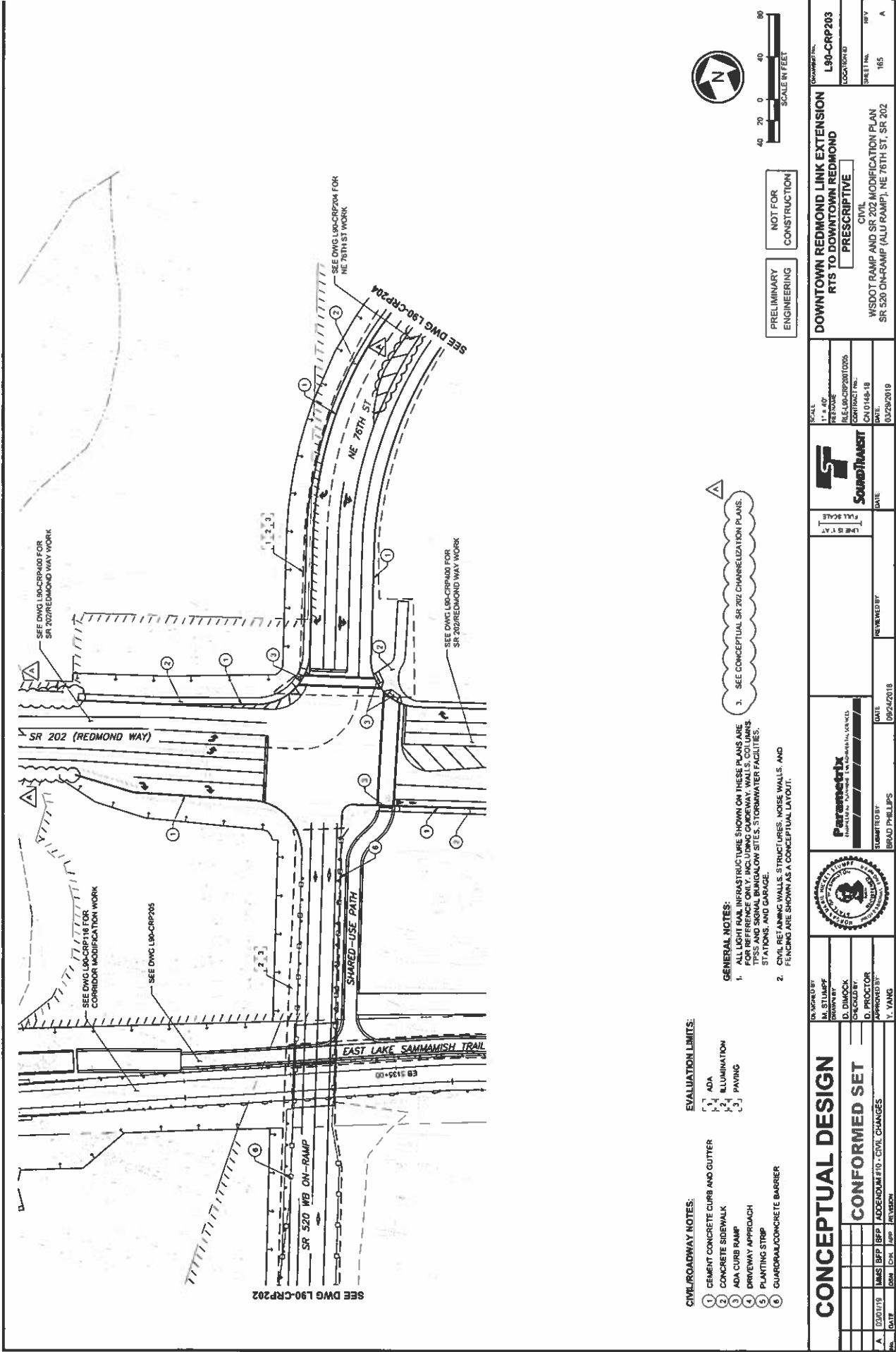
cc: Leonard McGhee (Project Manager, ST)  
ST Document Control



**ATTACHMENT 1**  
**SR 202 & NE 76TH STREET ROADWAY IMPROVEMENTS**  
**SOUND TRANSIT**  
DOWNTOWN REDMOND LINK EXTENSION

- LEGEND**
- SOUND TRANSIT WORK
  - CITY OF REDMOND NEW CHANNELIZATION WORK (BETTERMENT)
  - CITY OF REDMOND CIVIL WORK (BETTERMENT)





GENERAL NOTES:  
1. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TIES AND SIGNAL BUNGALOW SITES, STORMWATER FACILITIES, STATIONS, AND GARAGE.  
2. CIVIL RETAINING WALLS, STRUCTURES, NOISE WALLS, AND FENCING ARE SHOWN AS A CONCEPTUAL LAYOUT.

CIVIL/ROADWAY NOTES:

- 1 CEMENT CONCRETE CURB AND GUTTER
- 2 CONCRETE SIDEWALK
- 3 ADA CURB RAMP
- 4 DRIVEWAY APPROACH
- 5 PLANTING STRIP
- 6 GUARDRAIL/CONCRETE BARRIER

EVALUATION LIMITS:

- 1 ADA
- 2 ILLUMINATION
- 3 PAVING

CONCEPTUAL DESIGN

CONFORMED SET

DATE	BY	CHK	APP	REV
12/01/19	JMS	BPT	BPT	ADDENDUM 1.0 - CIVIL CHANGES

DESIGNED BY M. STUMP	REVIEWED BY D. LINDEN
DRIVEN BY O. PROCTOR	APPROVED BY Y. YANG

PARAMETRIX CORPORATION 10000 15th Avenue NW Suite 100 Seattle, WA 98148-3199 TEL: 206.464.1000 WWW.PARAMETRIX.COM	DATE 09/24/2018
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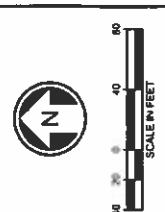
REVIEWED BY DATE	REVIEWED BY DATE
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SCALE 1" = 40' 1/4" = 10'	DATE 03/29/2018
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PROJECT NO. L90-CRP203	DATE 03/29/2018
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PROJECT NO. L90-CRP203	DATE 03/29/2018
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PROJECT NO. L90-CRP203	DATE 03/29/2018
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3. SEE CONCEPTUAL SR 202 CHANNELIZATION PLANS.

**GENERAL NOTES:**

1. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TIES AND SIGNAL BUNGALOW SITES, STORMWATER FACILITIES, STATIONS, AND GARAGE.
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**CIVILROADWAY NOTES:** **EVALUATION LIMITS:**

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- 2 CONCRETE SIDEWALK
- 3 ADA CURB RAMP
- 4 DRIVEWAY APPROACH
- 5 PLANTING STRIP
- 6 GUARDRAIL/CONCRETE BARRIER

**EVALUATION LIMITS:**

- 1 AOA  
2 ILLUMINATION  
3 PAVING

## CONCEPTUAL DESIGN

**CONFORMED SET**

[illegible]

DESIGNED BY:	M. STUMPF
DRAWN BY:	D. DIMOCK
CHECKED BY:	D. PROCTOR
APPROVED BY:	Y. YANG



## Parametrix

Submitted by  
BRAD PHILLIPS

RECEIVED BY

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SCALE

30.24 E  
1.2 = 40%

04-1  
↑ E.E. NAME  
9002002685706 F318

CONTRACT NO.

DATE \_\_\_\_\_  
BY \_\_\_\_\_

**DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND**

## **DOWNTOWN RE PRESS**

CIVIL  
CITY OF REDMOND ROADWAY MODIFICATION PLAN  
NE 78TH ST

164






PRELIMINARY  
ENGINEERING

DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND  
[REDACTED] PREScriptive  
CIVIL  
WSDOT MODIFICATION PLAN  
SR 202, REDMOND WY

SCALE  
1" = 40'  
BY E. NAME:  
FILE NO. CRP40070402  
CONTRACT NO.:  
CN 0148-18  
DATE  
03/29/2019



	DRAWN BY DATE	2018	REVIEWED BY	3
				LINE IS 1:1 AT FULL SCALE



DE. SECRETED BY  
 M. STUMPF  
 DRAWN BY  
 D. DIMOCK  
 CHECKED BY  
 D. PROCTOR  
 APPROVED BY  
 Y. YANG

[illegible]

**CIVIL/ROADWAY NOTES:**

**CIVILROADWAY NOTES:**

1	CEMENT CONCRETE CURB AND GUTTER
2	CONCRETE SIDEWALK
3	ADA CURB RAMP
4	DRIVEWAY APPROACH
5	PLANTING STRIP
6	GUARDRAIL/CONCRETE BARRIER

EVALUATION	ADA	ILLUM	PAYIN
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2			
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**GENERAL NOTES:**

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  2. CIVIL RETAINING WALLS, STRUCTURES, NOISE WALLS, AND FENCING ARE SHOWN AS A CONCEPTUAL LAYOUT.


3. SEE CONCEPTUAL SR 202 CHANNELIZATION PLANS.

PRELIMINARY  
ENGINEERING

DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND  
[REDACTED] PREScriptive  
CIVIL  
WSDOT MODIFICATION PLAN  
SR 202, REDMOND WY

SCALE  
1" = 40'  
BY E. NAME:  
FILE NO. CRP40070402  
CONTRACT NO.:  
CN 0148-18  
DATE  
03/29/2019

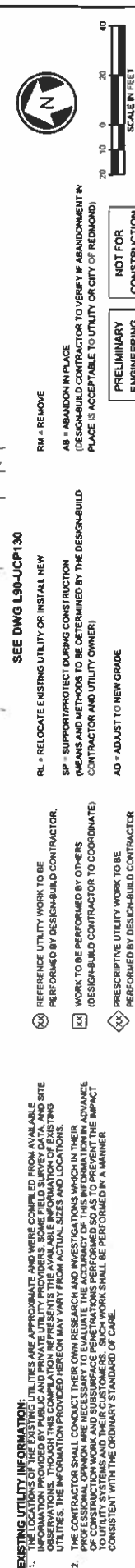


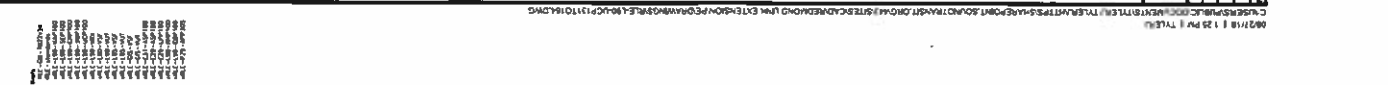
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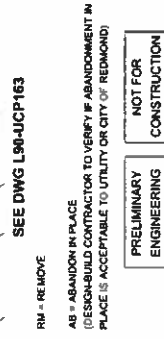


DE. SECRETED BY  
 M. STUMPF  
 DRAWN BY  
 D. DIMOCK  
 CHECKED BY  
 D. PROCTOR  
 APPROVED BY  
 Y. YANG

[illegible]

[illegible]





- CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
- ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS FOR INFORMATION ONLY. CONTRACTOR TO COORDINATE AND PERFORM ALL WORK IN ACCORDANCE WITH THE CITY OF CHICAGO AND ALL ORDINANCES AND REGULATIONS WITH EACH UTILITY PROVIDER.
- SEE DWGS. 100-SEP-100 TO 100-SEP-164 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- SEE DWGS. 100-AFP-100 TO 100-AFP-118 AND 100-AFX-101 TO 100-AFX-117 FOR TRACKWORK AND TRACK SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- SEE DWGS. 100-CRP-100 TO 100-CRP-168 FOR CROWNED ROADWAY AND ROADWAY SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- SEE DWGS. 100-CDP-100 TO 100-CDP-168 FOR STORM DRAINAGE AND SURFACE WATER DRAINAGE INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- SEE DWGS. 100-SEP-100 TO 100-SEP-118 AND 100-SY-100 TO 100-SY-168 FOR STRUCTURAL COLUMN AND WALL WITH FOUNDATION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

RM = RE MOVE  
AB = ABANDON IN PLACE  
(DESIGN-BUILD CONTRACTOR TO VERIFY IF ABANDONMENT IN PLACE IS ACCEPTABLE TO UTILITY OR CITY OF REDMOND)

PRELIMINARY  
ENGINEERING

CONTRACTOR AND UTILITY OWNER)

0 = ADJUST TO NEW GRADE

☒ REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.

☒ WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)

PRESCRIPTIVE UTILITY WORK TO BE

1. THE LOCATIONS OF THE EXISTING UTILITIES ARE APPROXIMATE AND WERE COMPILED FROM AVAILABLE INFORMATION PROVIDED BY PUBLIC AND PRIVATE UTILITY PROVIDERS. SOME FIELD SURVEY DATA, AND SITE OBSERVATIONS, THROUGH THIS COMPLETION REPRESENTS THE AVAILABLE INFORMATION OF EXISTING UTILITIES. THE INFORMATION PROVIDED HEREON MAY VARY FROM ACTUAL SIZES AND LOCATIONS.
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[illegible]

1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.

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3. SEE DWGS U00-APP-100 TO U00-APP-110 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

4. SEE DWGS U00-APP-100 TO U00-APP-110 AND U00-APP-101 TO U00-APP-102 FOR TRAIL AND SIDE SECTION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

5. SEE DWGS U00-APP-100 TO U00-APP-102 FOR CUL/ROADWAY/LANES/CAP/SC/PIPE/LIN/UMINATION AND ROADWAY SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

6. SEE DWGS U00-APP-100 TO U00-APP-102 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

7. SEE DWGS U00-APP-100 TO U00-APP-110 FOR STRUCTURAL INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).

SEE DWG L90-UCP164

MS 7674 27

**EXISTING UTILITY INFORMATION:**  
THE LOCATIONS OF THE EXISTING UTILITIES ARE APPROXIMATE AND WERE COMPILED FROM AVAILABLE INFORMATION PROVIDED BY PUBLIC AND PRIVATE UTILITY PROVIDERS. SOME FIELD SURVEY DATA, AND SITE INFORMATION, INCLUDING BUT NOT LIMITED TO, EXISTING UTILITIES, AND EXISTING FOUNDATIONS, EXISTING UTILITIES, THE INFORMATION PROVIDED HEREON MAY VARY FROM ACTUAL SIZES AND LOCATIONS.

2. THE CONTRACTOR SHALL CONDUCT THEIR OWN RESEARCH AND INVESTIGATIONS WHICH IN THEIR PROFESSIONAL OPINION ARE NECESSARY TO EVALUATE THE ACCURACY OF THIS INFORMATION IN ADVANCE OF CONSTRUCTION WORK. AND SURFACE PENETRATIONS PERFORMED SO AS TO PREVENT THE IMPACT OF EXISTING UTILITIES ON THE PROPOSED CONSTRUCTION WORK SHALL BE PERFORMED IN A MANNER CONSISTENT WITH THE PRIMARY STAYWARD OF CARE.

XX	REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.
XX	WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)
XX	PRESCRIPTIVE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR

SP = SUPPORT/PROTECT DURING CONSTRUCTION  
MEANS AND METHODS TO BE DETERMINED BY THE  
CONTRACTOR AND UTILITY OWNER)

RM = REMOVE  
AB = ABANDON  
[DESIGN-BUILD  
PLACE IS ACCE

AD = ADJUST TO NEW GRADE

NOT FOR  
PRELIMINARY  
CONSTRUCTION  
ENGINEERING

**NOT FOR  
CONSTRUCTION**



## CONCEPTUAL DESIGN

**CONFORMED SET**

DESIGNED BY  
M. AUSTIN  
DRAWN BY  
J. J. BETZVOG  
CHECKED BY  
D. PROCTOR  
APPROVED BY  
X. YANG

M. AUSTIN  
CRAWFORD III

**J. BETZVOG**

D. PROCTOR

01011111



•

**WATERBURY**

1

48 (3) 1998



**SOUND TRANSITIONS**  
A/E.

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SCALE	1" = 20'
ELEMENT	FILE-100-UCP13110164
CONTRACT No.	CN 0148-18
DATE	03/29/2019

**Scale**

1° - 20°

FILE-L90-UCP131T0164

CN 0143-18

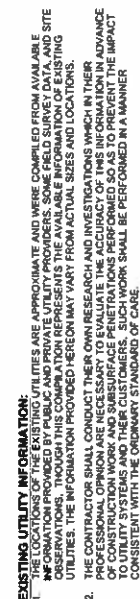
01/07/2010  
DATE




## DOWNTOWN REDMOND LINK EXTENSION RTS TO DOWNTOWN REDMOND

## REFERENCE

COMPOSITE UTILITIES PLAN

1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
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4. SEE DWGS L&W-CIP-100 TO L&W-CIP-119 AND L&W-CIP-101 TO L&W-CIP-117 FOR TRACKWORK AND TRACK SECTOR INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
5. SEE DWGS L&W-CIP-100 TO L&W-CIP-102 FOR CHILDRoadWAYS, LANDSCAPING, ILLUMINATION AND ROADWAY RELOCATION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
6. SEE DWGS L&W-CIP-100 TO L&W-CIP-102 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
7. SEE DWGS L&W-SEP-100 TO L&W-SEP-119 AND L&W-SY-100 TO L&W-SY-103 FOR STRUCTURAL COLUMN AND WALL INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).



 REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.  
 WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)  
 PRESCRIPTIVE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR


AD = ADJUST TO NEW GRADE  
SUPPORT/PROTECT DURING CONSTRUCTION  
MEANS AND METHODS TO BE DETERMINED BY THE CONTRACTOR AND UTILITY OWNER  
RELOCATE EXISTING UTILITY OR INSTALL NEW

NEW  
TION  
BY THE DESIGN-BUILD

RM = REMOVE  
AB = ABANDON  
RDESIGN-BUILD  
PLACE IS ACCF



NOT FOR  
PRELIMINARY  
CONSTRUCTION  
ENGINEERING

CONCEPTUAL DESIGN	DATE: 11/11/2019	PROJECT: 1909UCP164	SCALE: 1" = 20'	 <b>SOUND TRANSIT</b>	SCALE 1" = 20' DATE: 11/11/2019	PROJECT: 1909UCP164 SCALE 1" = 20' DATE: 11/11/2019	DOWNTOWN REDMOND LINK EXTENSION RTS TO WESTERN REDMOND
	CONFORMED SET	DATE: 11/11/2019	PROJECT: 1909UCP164				








A horizontal scale bar labeled "SCALE IN FEET" with markings at 0, 10, 20, and 40.

PRELIMINARY  
ENGINEERING

**DOWNTOWN REDMOND LINK EXTENSION**  
**RTS TO DOWNTOWN REDMOND**  
 REFERENCE: CIVIL  
 DRAINAGE PLAN  
 SR 202/REDMOND WY & SR 520

SCALE
1" = 20'
FILE NAME
RUE-490-CDP10010162
CONTRACT NO.
CN 0148-18
DATE
03/29/2019



2018	MEVE WRODSY		LINE 15 - AT FULL SCALE
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DESIGNED BY	T. NABOURS
DRAWN BY	J. TYLER
CHECKED BY	J. HITCH
APPROVED BY	Y. YANG

<b>SIGN</b>	
<b>D SET</b>	

**QUAL DE CONFORME**

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**CONSTRUCTION NOTES:**

1. CONNECT TO EXISTING STORM DRAIN STRUCTURE.
2. CONNECT TO EXISTING STORM DRAIN PIPE.
3. PROTECT EXISTING STORM DRAIN FACILITY.
4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GROUND.

**CONST** **①** **CON**

**NOTES:**

1. SEE DRAWING L90-C2N700 FOR GENERAL NOTES.
2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY INCLUDING GUIDEWAY, WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

## CONCEPTUAL DESIGN

**CONFORMED SET**

IRE VISION





## EXHIBIT C

February 5th, 2019

Kristi Wilson  
Interim Director of Public Works  
City of Redmond  
15670 NE 85<sup>th</sup> Street  
Redmond, WA, 98073

**Downtown Redmond Link Extension, Letter of Concurrence**  
**LOC 17 DRLE: NE 70<sup>th</sup> Street & Redmond Way (SR 202) Betterment and Right of Way (ROW) Acquisition & Schedule**

### Purpose

The City of Redmond requests that Sound Transit construct, as a betterment, an additional northbound left turn lane at the intersection of Redmond Way and NE 70<sup>th</sup> Street. The purpose of this letter is to seek concurrence from the City of Redmond on the funding responsibility, civil improvements, and right-of-way (ROW) acquisition associated with this betterment.

### Redmond Way Roadway Improvements

The scope of the City betterment at Redmond Way and NE 70<sup>th</sup> St. is:

- Design and construction of an additional northbound left turn lane at the intersection of SR 202 and NE 70<sup>th</sup> Street.
- Widening of SR 202 to the northeast to accommodate the additional left turn lane
- Realignment of through lanes along Redmond Way east and west of NE 70<sup>th</sup> Street required because of the additional left turn lane.
- Modifications to the existing medians within Redmond Way required because of the additional left turn lane
- Stormwater and drainage modifications required to accommodate the additional left turn lane (on-site or modifications to an existing pond, to be determined by Sound Transit's design-build contractor)
- Signal and lighting modifications required because of the additional left turn lane
- Any additional work required solely to complete the betterment

Construction of the additional right turn lane from SB Redmond Way to WB NE 70<sup>th</sup> St., and any additional work required solely to complete the right turn lane, is excluded from the scope of the betterment. The City of Redmond agrees to pay for improvements related to the betterment. See Attachment 2 for the proposed civil improvements related to the addition of the northbound left turn lane.

#### CHAIR

**John Marchione**  
*Redmond Mayor*

#### VICE CHAIRS

**Ron Lucas**  
*Steilacoom Mayor*

**Paul Roberts**  
*Everett Council President/  
Mayor Pro Tem*

#### BOARD MEMBERS

**Nancy Backus**  
*Auburn Mayor*

**David Baker**  
*Kennmore Mayor*

**Claudia Balducci**  
*King County Councilmember*

**Dow Constantine**  
*King County Executive*

**Bruce Dammeier**  
*Pierce County Executive*

**Jenny Durkan**  
*Seattle Mayor*

**Dave Earling**  
*Edmonds Mayor*

**Rob Johnson**  
*Seattle Councilmember*

**Kent Keel**  
*University Place Mayor*

**Joe McDermott**  
*King County Council Chair*

**Roger Millar**  
*Washington State Secretary  
of Transportation*

**Dave Somers**  
*Snohomish County Executive*

**Dave Upthegrove**  
*King County Councilmember*

**Peter von Reichbauer**  
*King County Councilmember*

**Victoria Woodards**  
*Tacoma Mayor*

#### CHIEF EXECUTIVE OFFICER

**Peter M. Rogoff**

Kristi Wilson  
February 5<sup>th</sup>, 2019  
Page 2

### Roadway Cross Sections

There are two different proposed cross sections for SR 202 in the vicinity of NE 70<sup>th</sup> Street. The details of the width of the roadway, sidewalks, planters, and bicycle facilities were determined in coordination with the City of Redmond. See Attachment 1 for these proposed cross sections. These sections are subject to final WSDOT channelization plan approval, to be completed by the Contractor.

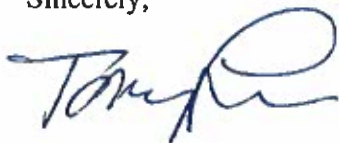
### Right of Way (ROW) Acquisition

The addition of the northbound left turn lane will have impacts on adjacent properties along the north side of SR 202.

Sound Transit agrees to produce the parcel maps and legal descriptions to support the right-of-way acquisitions. The City of Redmond will complete the necessary appraisals and acquire and purchase the property rights, both permanent and temporary. The targeted date for possession and use of the property is December 31, 2020, assuming receipt by the City of complete and accurate parcel maps and legal descriptions by April 1, 2019. Any delays past December 31, 2020 may incur additional costs due to change orders from the Contractor for which the City of Redmond will be responsible. If the City of Redmond is unable to provide possession and use by **January 31, 2022**, then Sound Transit will not construct the project.

Should you have any questions, please don't hesitate to contact me at (206) 398-5171. Please sign below and return one of the enclosed originals – please keep the additional copy for your records.

Sincerely,



Tony Raben  
Executive Project Director, DRLE

Concurrence:

Kristi Wilson, 2/19/19  
Date

Kristi Wilson, Interm Director of Public Works  
City of Redmond

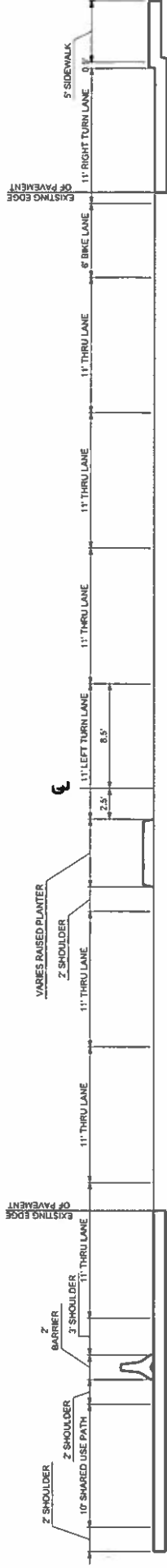
#### Attachments:

1. SR 202 Cross Sections
2. SR 202 Roadway Improvements

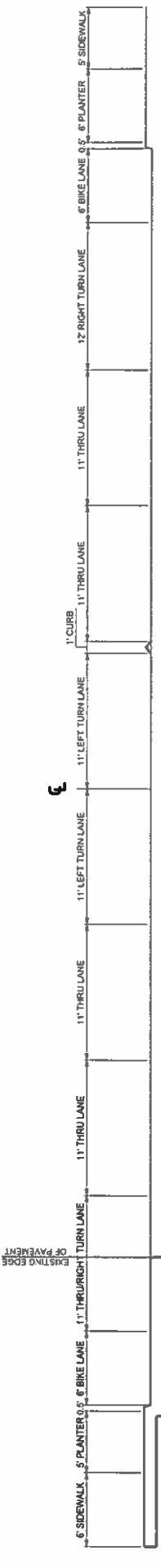
#### Enclosure (s):

cc: Leonard McGhee (Project Manager, ST)

ST Document Control



SR 202 TYPICAL SECTION  
A  
NORTH OF NE 10TH STREET  
NTS



SR 202 TYPICAL SECTION  
B  
SOUTH OF NE 70TH STREET  
NTS

**LEGEND**

EXISTING ROADWAY

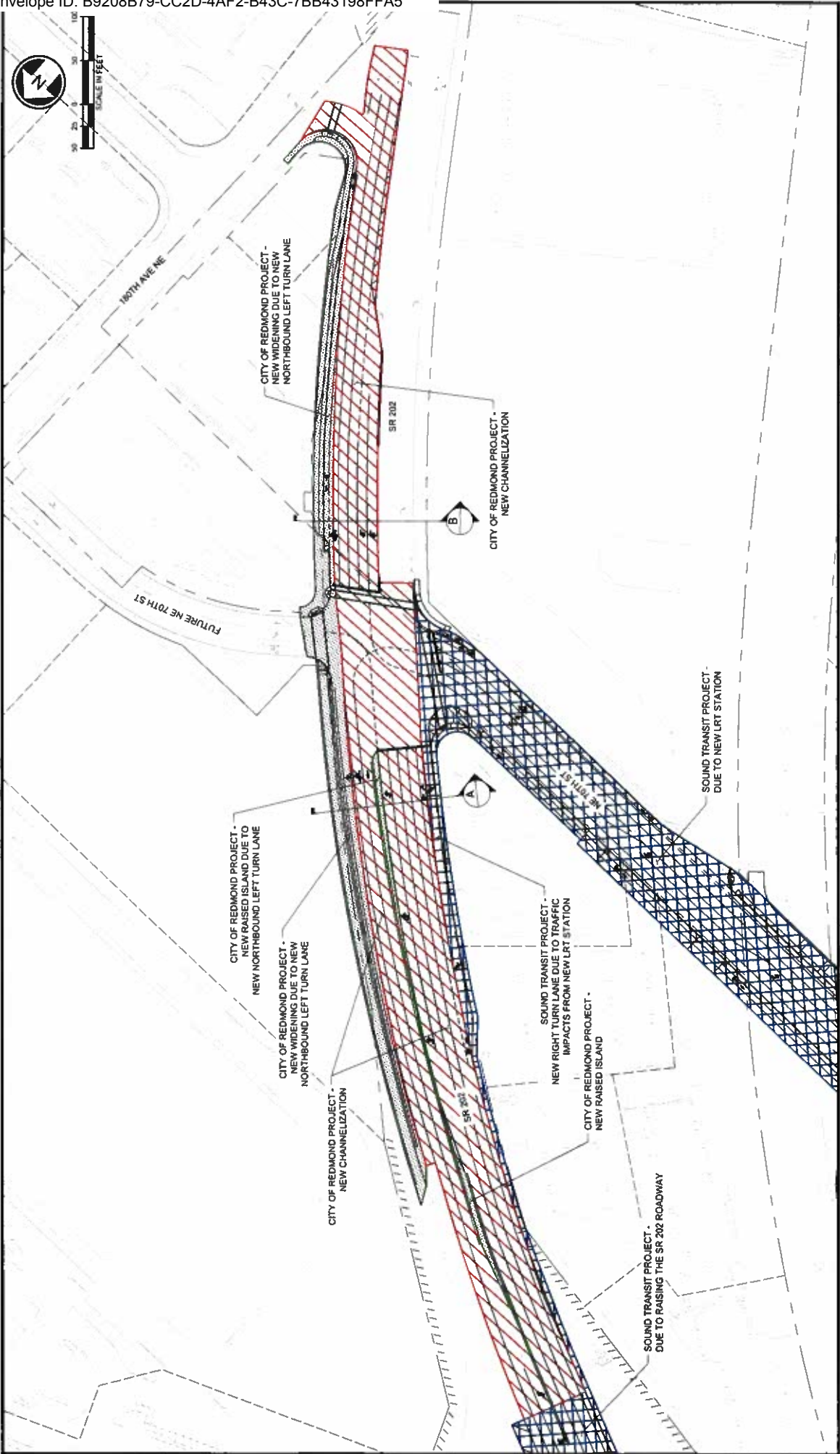
NEW ROADWAY

**ATTACHMENT 1**

**SR 202 CROSS SECTIONS**

**SOUNDTECH**

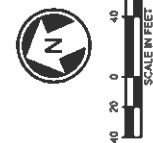
DOWNTOWN REDMOND LINK EXTENSION



LEGEND


- SOUND TRANSIT WORK
- CITY OF REDMOND CIVIL WORK


















































CITY OF REDMOND NEW CHANNELIZATION WORK



PRELIMINARY  
ENGINEERING

DOWNTOWN REDMOND LINK EXTENSION  
 RTS TO DOWNTOWN REDMOND  
 PRESCRIPTIVE  
 CIVIL  
 WSDOT MODIFICATION PLAN  
 SR 202 / REDMOND WY  
 L90-CRP401  
 (project) (file)  
 LOCAL FILE ID  
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FILE NO. C89-00370402	
CONTRACT NO.	
CN 0146-18	

REVIEWED BY	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
	
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**PARAMETRIX**  
 10000 10th Avenue, Suite 100, San Diego, CA 92121  
 (619) 591-1000

**DATE** \_\_\_\_\_

**EXAMINATED BY** \_\_\_\_\_

DR. SUGAR O OY  
M. STUMPF  
DRAWN BY  
D. DIMOCK  
CHECKED BY  
D. PROCTOR  
APPROVED BY

[illegible]

**CIVIL ROADWAY NOTES:**

- |   |                                 |   |              |
|---|---------------------------------|---|--------------|
| 1 | CEMENT CONCRETE CURB AND GUTTER | 1 | ADA          |
| 2 | CONCRETE SIDEWALK               | 2 | ILLUMINATION |
| 3 | ADA CURB RAMP                   | 3 | PAVING       |
| 4 | DRIVEWAY APPROACH               |   |              |
| 5 | PLANTING STRIP                  |   |              |
| 6 | GUARDRAIL/CONCRETE BARRIER      |   |              |

**GENERAL NOTES:**

1. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TPSS AND SIGNAL BUNGALOW SITES, STORMWATER FACILITIES, STATIONS, AND GARAGE.
2. CIVIL RETAINING WALLS, STRUCTURES, NOISE WALLS, AND FENCING ARE SHOWN AS A CONCEPTUAL LAYOUT.

3. SEE CONCEPTUAL SR 202 CHANNELIZATION PLANS.

## CONCEPTUAL DESIGN

**CONFORMED SET**

APPENDIX #10 - CIVIL CHANGES

DATE	CHK	ALPH	REVISION
Y. YANG			




00000118 | 11/27/11 | 11/27/11





1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
2. ALL ALLOCATION WORK SHOWN IN THESE DRAWINGS IS APPROXIMATE ONLY. CONTRACTOR TO COORDINATE AND DETERMINE EXACT LOCATIONS AND CONFIGURATIONS WITH EACH UTILITY PROVIDER.
3. SEE DWGS L90-SEP100 TO L90-SEP140 FOR RIGHT-OF-WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
4. SEE DWGS L90-CAP100 TO L90-CAP140 FOR TRUCKWAY AND TRUCK EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
5. SEE DWGS L90-CRP100 TO L90-CRP142 FOR CONCRETE PAVEMENT, CONCOURSE, LUMINA, AND ROADWAY SECTION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
6. SEE DWGS L90-CDP100 TO L90-CDP142 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
7. SEE DWGS L90-SEP100 TO L90-SEP119 AND L90-SY001 TO L90-SY003 FOR STORMWATER MANAGEMENT AND UTILITY STRUCTURAL SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).



 REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.  
 WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)  
 PRESCRIPTIVE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR

SSP = SUPPORT/PROTECT DURING CONSTRUCTION  
(MEANS AND METHODS TO BE DETERMINED BY THE DESIGN-BUILD  
CONTRACTOR AND UTILITY OWNER)

1.8 = ABANDON IN PLACE  
DESIGN-BUILD CONTRACTOR TO VERIFY IF ABANDONMENT IN  
PLACE IS ACCEPTABLE TO UTILITY OR CITY OF REDMOND)

[illegible]

DATE	10/24
SUBMITTED BY	IRAC PHILIPS



## SOUND TRANSIT

DATE	DATE
03/28/2019	03/28/2019

**DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND**

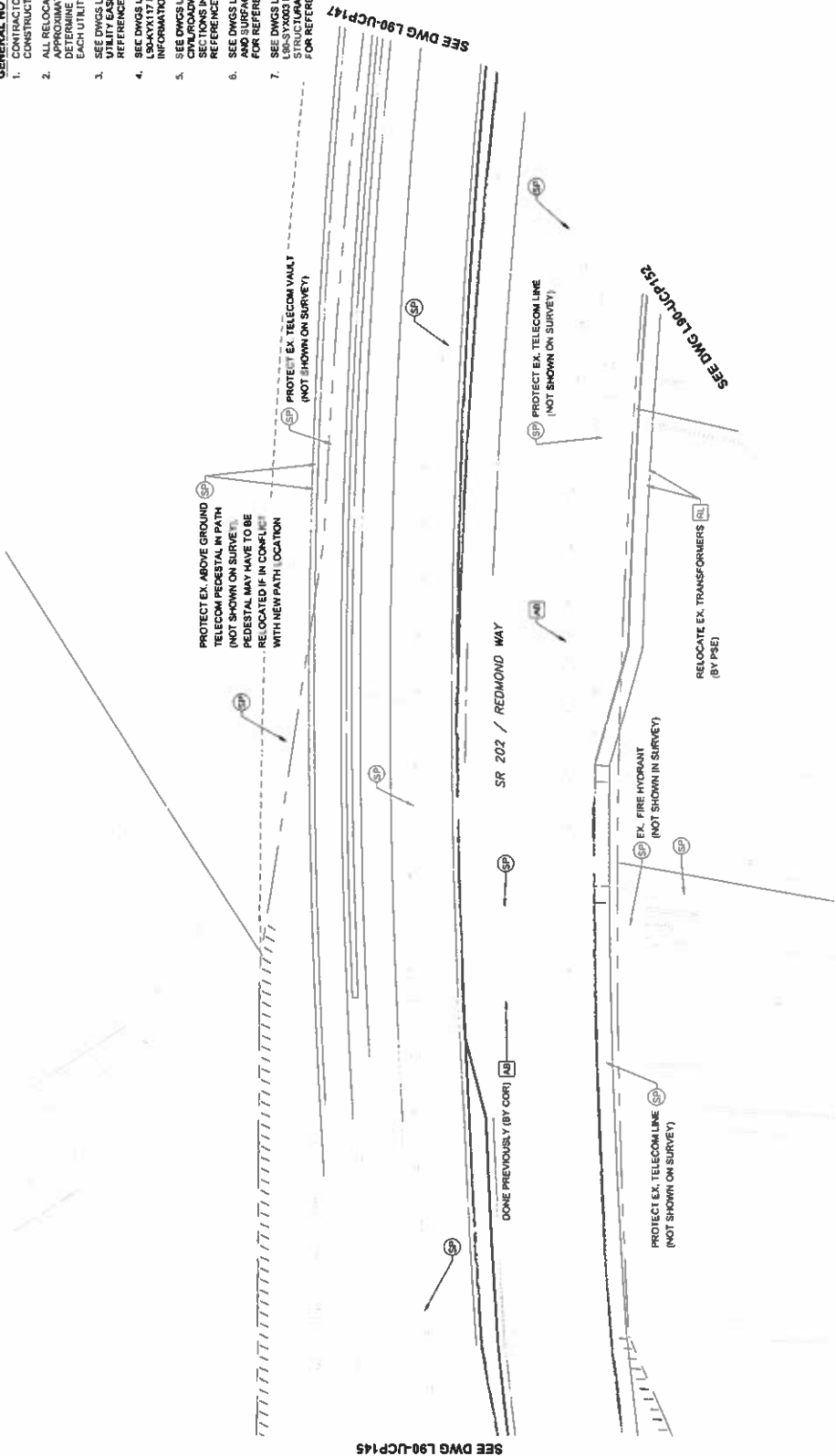
## REFERENCE

**COMPOSITE UTILITIES PLAN**

DATE	DATE	ONE OF PAGE 10
	03/28/2019	

GENERAL NOTES:

- 1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
- 2. ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS APPROXIMATE ONLY. CONTRACTOR TO COORDINATE AND DETERMINE EXACT LOCATIONS AND CONFIGURATIONS WITH EACH UTILITY PROVIDER.
- 3. SEE DWGS L90-RPP100 TO L90-RPP104 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- 4. SEE DWGS L90-SEP100 TO L90-SEP104 AND L90-SEP105 TO L90-SEP107 FOR TRACED WORK AND TRACK SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- 5. SEE DWGS L90-SEP100 TO L90-SEP102 FOR CIVIL/ROADWAY/ILLUMINATION AND ROADWAY SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- 6. SEE DWGS L90-SEP100 TO L90-SEP102 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
- 7. SEE DWGS L90-SEP100 TO L90-SEP103 AND L90-SEP104 TO L90-SEP107 FOR STRUCTURAL COLUMN AND WALL WITH FOUNDATION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).



**EXISTING UTILITY INFORMATION:**  
THE LOCATIONS OF THE EXISTING UTILITIES ARE APPROXIMATE AND WERE COMPILED FROM AVAILABLE INFORMATION PROVIDED BY PUBLIC AND PRIVATE UTILITY PROVIDERS. SOME FIELD SURVEY DATA, AND SITE OBSERVATIONS, WERE USED TO CORRELATE THESE DATA WITH THE AVAILABLE INFORMATION. THE EXISTING UTILITIES ARE SHOWN AS APPROXIMATE LOCATIONS. THE CONTRACTOR SHALL CONDUCT THEIR OWN RESEARCH AND INVESTIGATIONS WHICH IN THEIR PROFESSIONAL OPINION ARE NECESSARY TO EVALUATE THE ACCURACY OF THIS INFORMATION IN ADVANCE OF CONSTRUCTION WORK AND SUBSURFACE PENETRATIONS PERFORMED SO AS TO PREVENT THE IMPACT TO UTILITY SYSTEMS. IF THEIR CUSTOMERS OR OTHERS HAVE BEEN ADVISED OF THE LOCATION OF ANY UTILITIES, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF SUCH UTILITIES IN A MANNER CONSISTENT WITH THE ORDINARY STANDARD OF CARE.

- RM = REMOVE
- AB = ABANDON IN PLACE
- SP = SUPPORT/PROTECT DURING CONSTRUCTION (MEANS AND METHODS TO BE DETERMINED BY THE DESIGN-BUILD CONTRACTOR AND UTILITY OWNER)
- AD = ADJUST TO NEW GRADE

RELOCATE EX. TRANSFORMERS (BY PSE)  
EX. FIRE HYDRANT (NOT SHOWN IN SURVEY)  
PROTECT EX. TELECOM LINE (NOT SHOWN ON SURVEY)  
PROTECT EX. TELECOM VAULT (NOT SHOWN ON SURVEY)

SR 202 / REDMOND WAY  
DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND

SEE DWG L90-UCP145  
SEE DWG L90-UCP147  
SEE DWG L90-UCP152



PRELIMINARY ENGINEERING  
NOT FOR CONSTRUCTION

<b>CONCEPTUAL DESIGN</b>		<b>CONFORMED SET</b>		<b>DATE</b>		<b>BY</b>	
DESIGNED BY: M. AUSTIN		CHECKED BY: J. BEITZOG		APPROVED BY: D. PROCTOR		V. YANG	
DRAWN BY: M. AUSTIN		CHECKED BY: J. BEITZOG		APPROVED BY: D. PROCTOR		V. YANG	
DATE: 08/24/2018		DATE: 08/24/2018		DATE: 08/24/2018		DATE: 08/24/2018	
SUBMITTED BY: BROAD PHILLIPS		DATE: 08/24/2018		DATE: 08/24/2018		DATE: 08/24/2018	
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DRAWING NO: L90-UCP146		DRAWING NO: L90-UCP146		DRAWING NO: L90-UCP146		DRAWING NO: L90-UCP146	

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1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
2. ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS APPROXIMATE ONLY. CONTRACTOR TO COORDINATE AND VERIFY ALL EXISTING LOCATIONS AND CONFIGURATIONS WITH EACH UTILITY PROVIDER.
3. SEE DWGS L90-DIP-100 TO L90-DIP-119 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
4. SEE DWGS L90-DIP-100 TO L90-DIP-119 AND L90-DIP-101 TO L90-DIP-117 FOR TRACKWORK AND TRACK SECTIONS.
5. SEE DWGS L90-CRP-100 TO L90-CRP-402 FOR TRACKWORK, LANDSCAPE/SCULPTURAL ILLUMINATION AND ROADWAY SECTION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
6. SEE DWGS L90-CDP-100 TO L90-CDP-162 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
7. SEE DWGS L90-SEP-100 TO L90-SEP-119 AND L90-STX-001 TO L90-STX-003 FOR STRUCTURAL COLUMN AND WALL WITH STRUCTURAL SECTION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).



(100) REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.  
 (100) WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)  
 (100) PRESCRIPTIVE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR

100X REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.

100X WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)

xx  
PRESCRIPTIVE UTILITY WORK TO BE  
PERFORMED BY DESIGN-BUILD CONTRACTOR

SP = SUPPORT/PROTECT DURING CONSTRUCTION  
(MEANS AND METHODS TO BE DETERMINED BY THE CONTRACTOR AND UTILITY OWNER)

AD • ADJUST TO NEW GRADE

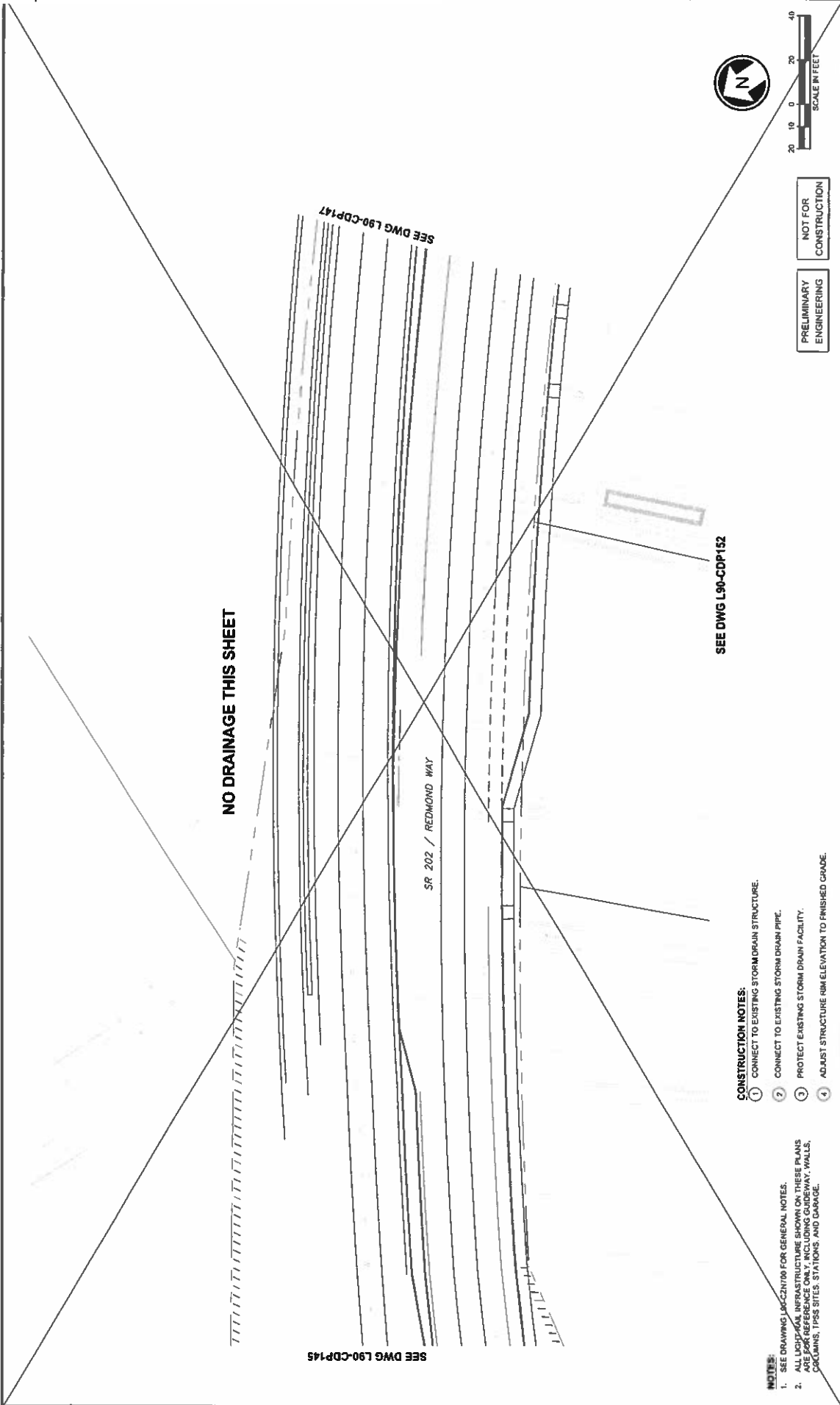
TERM = REMOVE

PRELIMINARY

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**NOTES:**

- 1. SEE DRAWING L90-CDP145 FOR GENERAL NOTES.
- 2. ALL LIGHT/PAVEMENT INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUTTER, WALLS, CURBS, TRIP SITES, STATIONS, AND GARAGE.

**CONSTRUCTION NOTES:**

- 1. CONNECT TO EXISTING STORM DRAIN STRUCTURE.
- 2. CONNECT TO EXISTING STORM DRAIN PIPE.
- 3. PROTECT EXISTING STORM DRAIN FACILITY.
- 4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE.

PRELIMINARY  
ENGINEERING

NOT FOR  
CONSTRUCTION



<b>CONCEPTUAL DESIGN</b>		<b>PARAMETRIX</b>		<b>SOUNDTRANSIT</b>		<b>DOWNTOWN REDMOND LINK EXTENSION RTS TO DOWNTOWN REDMOND</b>	
DESIGNED BY T. HARRIS	CHECKED BY J. TYLER	SUBMITTED BY BRAD PHILLIPS	DATE 09/24/2018	SCALE 1" = 20'	PROJECT NO. L90-CDP145	CONTRACT NO. CN 0148-18	DATE 03/29/2019
<b>CONFORMED SET</b>						<b>REFERENCE</b> CIVIL DRAINAGE PLAN SR 202/REDMOND WY	
						DRAWING NO. L90-CDP146	
						LOCATION ID	
						SHEET NO. 224	



A vertical scale bar labeled "SCALE IN FEET" is positioned on the right side of the map. It has markings at 20, 0, 20, and 40 feet, with the 0 mark in the center.


NOT FOR  
CONSTRUCTION

PRELIMINARY  
ENGINEERING

**DOWNTOWN REDMOND LINK EXTENSION**  
**RTS TO DOWNTOWN REDMOND**  
**REFERENCE**  
 CIVIL  
 DRAINAGE PLAN  
 SR 202/REDMOND WY & NE 70TH ST INTERSECTION

SCALE	1" = 20'
FILE NAME	RL-4.90-COP100T0482
CONTRACT NO.	CN 0148-18
DATE	03/28/2019

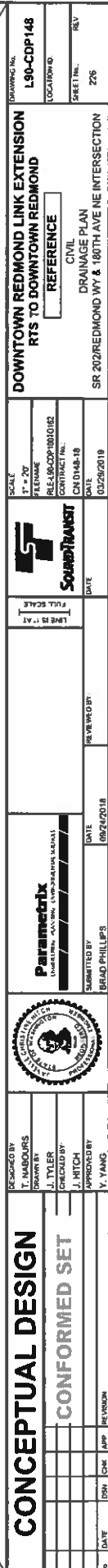


	DESIGNED BY
	T. NABOURS
	GRADUATE BY
	J. TYLER
	GRADUATE BY
J. HITCH	
APPROVED BY	
Y. YANG	

CONCEPTUAL DESIGN									
CONFORMED SET									
NO	DATE	ISSN	CHIL	JAPP	REVISION				

06/25/18 | 4:11 PM | TALK





SR 202 / REDMOND WAY

SEE DWG L90-CDP147

- ① CONNECT TO EXISTING STORM DRAIN STRUCTURE.
- ② CONNECT TO EXISTING STORM DRAIN PIPE.
- ③ PROTECT EXISTING STORM DRAIN FACILITY.
- ④ ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE.

**NOTES:**

1. SEE DRAWING L&CZNT700 FOR GENERAL NOTES.
2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE SHEETS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

[illegible]

DESIGNED BY	T. NABOURS
DRAWN BY	J. TYLER
CHECKED BY	J. HITCH
APPROVED BY	Y. YANG

## Parametrix

SUBMITTED BY	DATE
BRAD PHILLIPS	09/24/2018

DATE

	D



SCALE	1" = 20'
FILENAME	FILE-490-CDP10010162
CONTRACT No.:	CN 0148-18
DATE	03/29/2019

**DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND**

REFERENCE
CIVIL DRAINAGE PLAN SR 202/REDMOND WY & 180TH AVE NE INTERSECTION

PLANNING No.	L90-CDP148
LOCATION ID.	
HEI No.	226
REV	



## EXHIBIT D

March 6<sup>th</sup>, 2019

Kristi Wilson  
Interim Director of Public Works  
City of Redmond  
15670 NE 85<sup>th</sup> Street  
Redmond, WA, 98073

**Downtown Redmond Link Extension, Letter of Concurrence**  
**LOC 011 DRLE: Redmond Central Connector Trail and Stormwater Trunk Line**  
**from 164<sup>th</sup> Avenue to Bear Creek Trail, and Erratic Relocation**

### Purpose

The purpose of this letter is to seek concurrence from the City of Redmond on the Redmond Central Connector (RCC) Trail alignment and stormwater relocation from 164<sup>th</sup> Avenue NE to the existing Bear Creek Trail, relocation of the Erratic, and cost sharing for RCC corridor improvements.

### Redmond Central Connector

The City of Redmond has provided Sound Transit a 30% trail alignment for the relocated trail alignment from 164<sup>th</sup> Avenue NE to the Bear Creek Trail (attached). The revised alignment has been incorporated into the civil sheets of the RFP Conceptual Design Drawings. At parcel no. 1225059019, it is assumed that the RCC corridor is 100' wide. If RCC right of way adjacent to this parcel is reduced, the City of Redmond and Sound Transit will reevaluate the trail alignment and property acquisition at that time.

City of Redmond also provided guidance to the Contractor for required finishing, bands, mixing zones, and other treatments per the RCC Design Guidance (attached).

The Contractor shall use the following pavement depths west of 170<sup>th</sup> Ave NE to accommodate stormwater trunkline maintenance needs:

Pavement Type	Material
Asphalt	4.5" HMA over 4" crushed rock
Concrete	6" unreinforced concrete over 4" crushed rock

Modifications to the 30% trail alignment may be suggested by the final design team based on changes to other disciplines such as; column locations, curb line adjustments, or station design. Changes to the trail alignment must be approved by the City of Remond.

#### CHAIR

**John Marchione**  
*Redmond Mayor*

#### VICE CHAIRS

**Ron Lucas**  
*Steilacoom Mayor*

**Paul Roberts**  
*Everett Council President/  
Mayor Pro Tem*

#### BOARD MEMBERS

**Nancy Backus**  
*Auburn Mayor*

**David Baker**  
*Kenmore Mayor*

**Claudia Balducci**  
*King County Councilmember*

**Dow Constantine**  
*King County Executive*

**Bruce Dammeler**  
*Pierce County Executive*

**Jenny Durkan**  
*Seattle Mayor*

**Dave Earling**  
*Edmonds Mayor*

**Rob Johnson**  
*Seattle Councilmember*

**Kent Keel**  
*University Place Mayor*

**Joe McDermott**  
*King County Council Chair*

**Roger Millar**  
*Washington State Secretary  
of Transportation*

**Dave Somers**  
*Snohomish County Executive*

**Dave Upthegrove**  
*King County Councilmember*

**Peter von Reichbauer**  
*King County Councilmember*

**Victoria Woodards**  
*Tacoma Mayor*

#### CHIEF EXECUTIVE OFFICER

**Peter M. Rogoff**

Kristi Wilson  
March 6th, 2019  
Page 2

### **Stormwater Trunk Line**

The existing City of Redmond stormwater trunk line will be relocated due to station/column impacts from 164<sup>th</sup> Avenue NE to the east end of the Downtown Redmond Station, as well as where in conflict with the traction power substation near 170<sup>th</sup> Avenue NE. The center of pipe of the relocated stormwater trunk line will be located at a minimum 10' from the edge of guidway shaft or station building. Manholes must be located within 10' from the edge of the Redmond Central Connector trail, and where possible will not be located within the trail.

The City's standard is no increase in HGL. As part of PE, the City reviewed the reference design (see attachment) and found it to be an acceptable design solution with a small increase in HGL. The design builder will have to apply for a deviation if the standard cannot be met in final design.

Similar to the RCC trail alignment at parcel no. 1225059019, the stormwater trunk alignment may be reconsidered by Sound Transit and City of Redmond should right-of-way not be available.

### **Erratic Relocation**

The City owns an art piece called the Erratic, located near the southwest corner of 166<sup>th</sup> Ave NE and Cleveland St. The art must be relocated because its current location conflicts with the conceptual designs for the Downtown Redmond station area. The City and Sound Transit agree that the Erratic will be relocated to Gilman Landing consistent with the attached conceptual drawings. The City agrees to conduct the site preparation and relocation work under separate contract(s) so that the Erratic is removed from its existing location by March 31, 2020.

### **Funding of RCC Corridor Improvements**

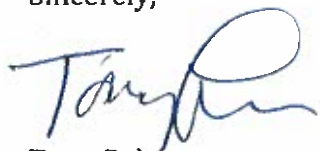
Subject to City Council and Sound Transit Board approval, City and Sound Transit staff have agreed to share in the funding of RCC corridor improvements as follows:

- Sound Transit will pay the City to relocate the Erratic and complete site preparation at Gilman Landing (finish design, permitting, construction) in the amount of \$576,300.
- Sound Transit will take full funding responsibility for RCC elements west of 166<sup>th</sup>
- Sound Transit will take full funding responsibility for signal improvements at Cleveland/166<sup>th</sup>
- Sound Transit will take full funding responsibility for RCC corridor landscaping, and will be allowed to count this as part of the tree mitigation plan
- The City and Sound Transit will evenly split funding responsibility for all other RCC elements east of 166<sup>th</sup>, including but not limited to:
  - The trail itself, including appropriate pavement depths to support stormwater maintenance requirements
  - Braids/ties/benches
  - Landings (apart from landscaping)
  - Conduit
  - Lighting
  - Any additional right-of-way costs at the SE corner of 166<sup>th</sup> Ave NE and Cleveland St
  - Storm water management facilities specifically required for the RCC trail (bioswales, raingardens, etc.)

Kristi Wilson  
March 6th, 2019  
Page 3



Should you have any questions, please don't hesitate to contact me at (206) 398-5171. Please sign below and return one of the enclosed originals – please keep the additional copy for your records.

Sincerely,



Tony Raben  
Executive Project Director, DRLE

Concurrence:

 /   
Date

Kristi Wilson, Interim Director of Public Works

City of Redmond

Attachments:

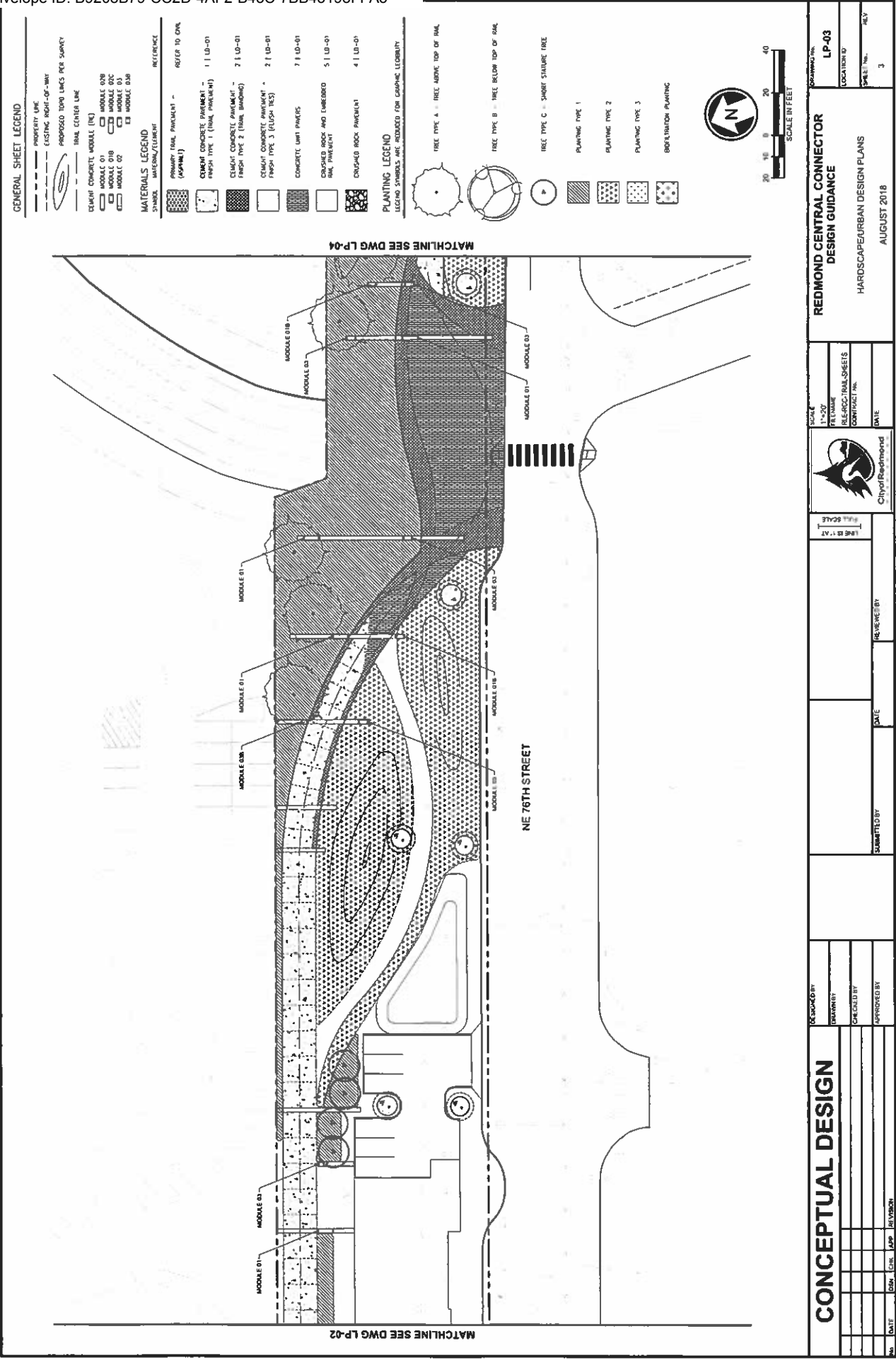
RCC Design Guidance drawings  
Gilman Landing Conceptual drawings  
PE Stormwater Trunkline design

Enclosure (s):

cc: Leonard McGhee (Project Manager, ST)  
ST Document Control















## PLANTING SCHEDULE

# PAVEMENTS / FLATWORK

SYMBOL	MATERIAL	REFERENCE	DESCRIPTION	FINISH	NOTES
	PRIMARY TRENCH PAVEMENT	REFER TO ONE	ASPHALT	STANDARD VDOT	ONE TO DRC
	CEMENT CONCRETE PAVEMENT - FINISH TYPE 1	1 1D-01	TYPICAL FINAL	LIGHT BROWN FINISH - PERPENDICULAR TO TRAVEL	SCORE PER SPECS & PLANS
	CEMENT CONCRETE PAVEMENT - FINISH TYPE 2	2 1D-04	TOTAL THICKNESS	MEDIUM BROWN FINISH - DIRECTION OF TRAVEL	COLOR CHARTS ARE ON EQUAL SCORE PER SPECS & PLANS
	CEMENT CONCRETE PAVEMENT - FINISH TYPE 3	2 1D-04	FLUSH RES.	LIGHT SANDBLAST	SCORE PER SPECS & PLANS
	CONCRETE UNIT PAVERS	1 1D-01	NOT TOPDRESS STANDARD	3 1/2" SIZE, COLORS: CHARCOAL, OIL, NATURAL (SEE SHADOW DET)	SEE PROJECT/PLAN FOR DETAIL, PATTERN PER DETAIL
	CURBED ROAD PAVEMENT	4 1D-01	1" MINUS	COLOR PER CITY OF REICHARD	
	CURBED ROAD AND EUROROAD RAIL PAVEMENT	5 1D-01	1" MINUS	COLOR PER CITY OF REICHARD	EMBEDDED RAILS REQUIRED AND PROVIDED BY CITY OF REICHARD ON SITE. IN SECURE LOCATION

# SITE FURNISHINGS

SYMBOL	MATERIAL	REFERENCE	DESCRIPTION	FINISH	NOTES
	PRECAST CONCRETE MODULE - MODULE 01	1 1D-03	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 01B	1 1D-03	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 02	2 1D-02	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 02B	2 1D-02	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 02C	2 1D-02	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 03	1 1D-04	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	PRECAST CONCRETE MODULE - MODULE 03B	1 1D-04	PRECAST CONCRETE MODULE	SMOOTH FINISH	LAYOUT & ELEVATIONS PER PLANS. FIELD VIEWS - W/ ENGINEER PRIOR TO PLACEMENT
	LIGHT & WOOD RAIL ON LIGHT AND WOOD RAIL MODULE ATTACHMENT	5 1D-04	ALUMINUM LUMEN LED LINEAR LIGHT WITH WOOD BEACH	REFER TO DETAIL	REFER TO ADDITIONAL PLANS, DETAILS AND SECTIONS IN RUC-94
	BARE ROCK	1 1D-02	FOOTING MOUNTED BARE ROCK	REFER TO DETAIL	NO SCRATCH
	TRASH RECEPTACLE		SURFACE LIGHT		PROVIDE 10" UNITS CITY TO LOCATE
	METAL EDGE	6 1D-01	4" HIGH C/STREET GAUGE, STEEL EDGE	SPECIFICATION 6.02.2	INSTALL WITH STAPLES AND CONNECTIONS PER DETAIL

## TRADES

[illegible]

## PLANTING MIXES

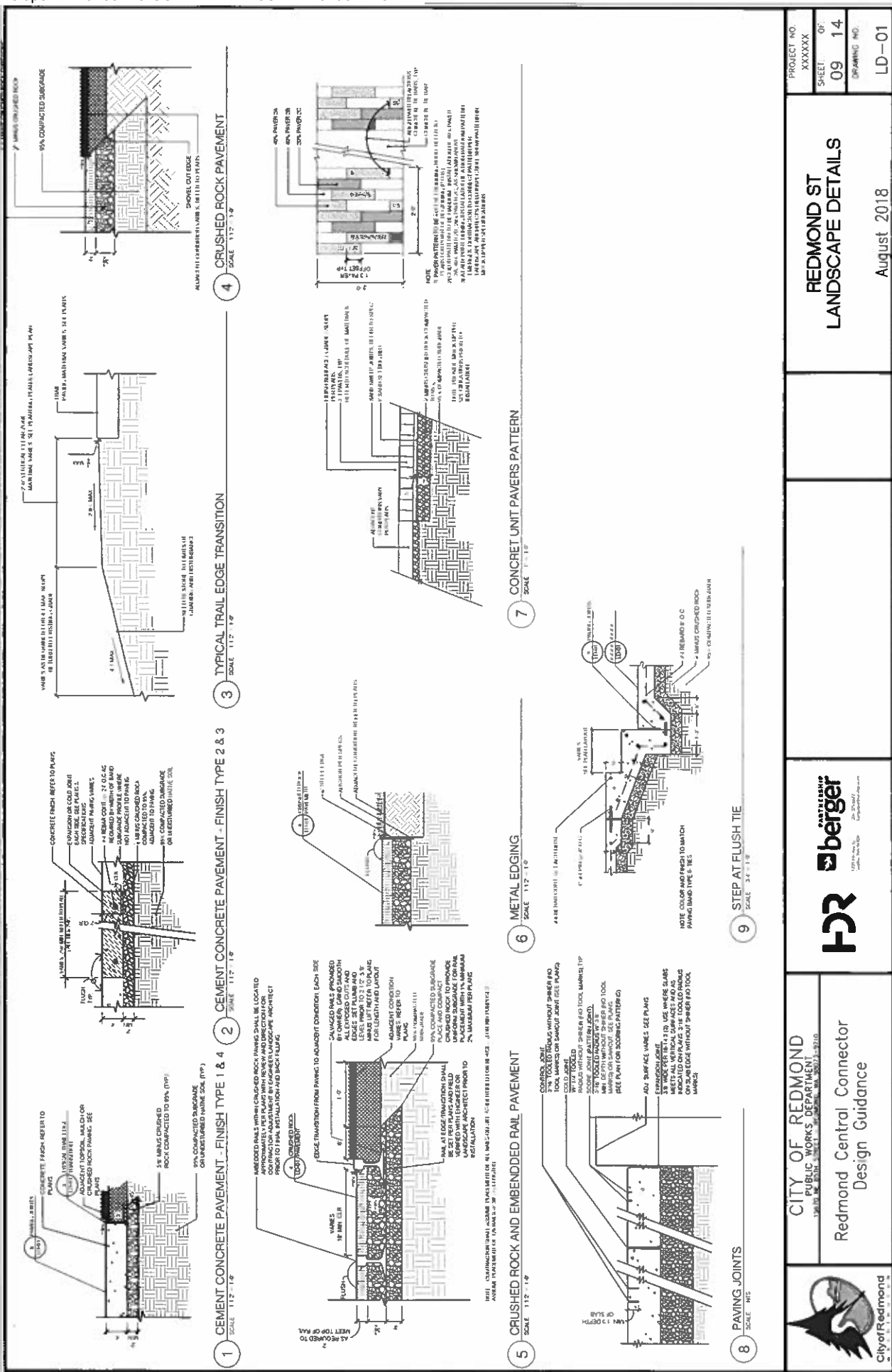
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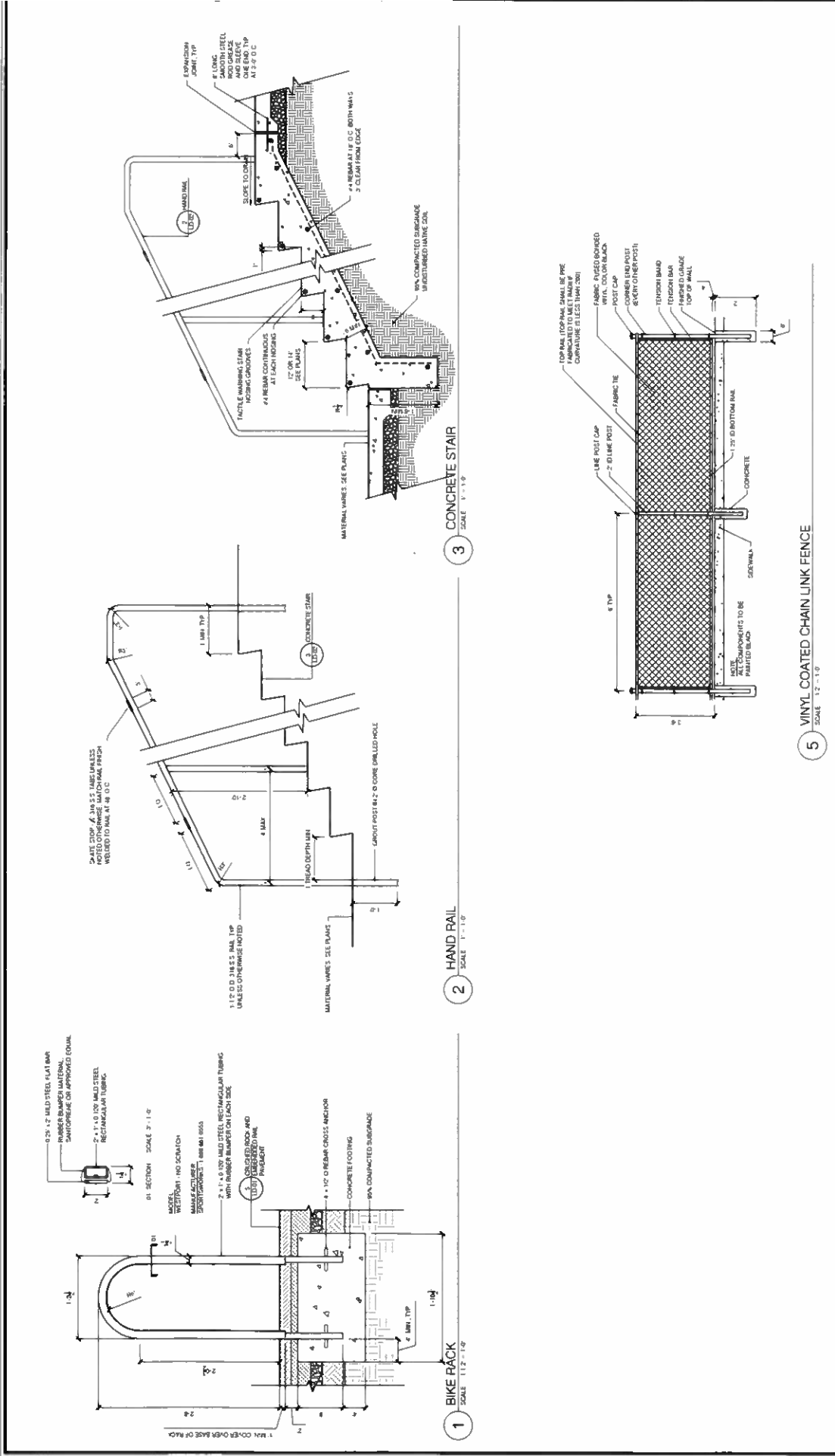
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1 GAL	24 °C
1 GAL	28 °C
1 GAL	24 °C
1 GAL	24 °C
1 GAL	30 °C
1 GAL	24 °C
1 GAL	35 °C



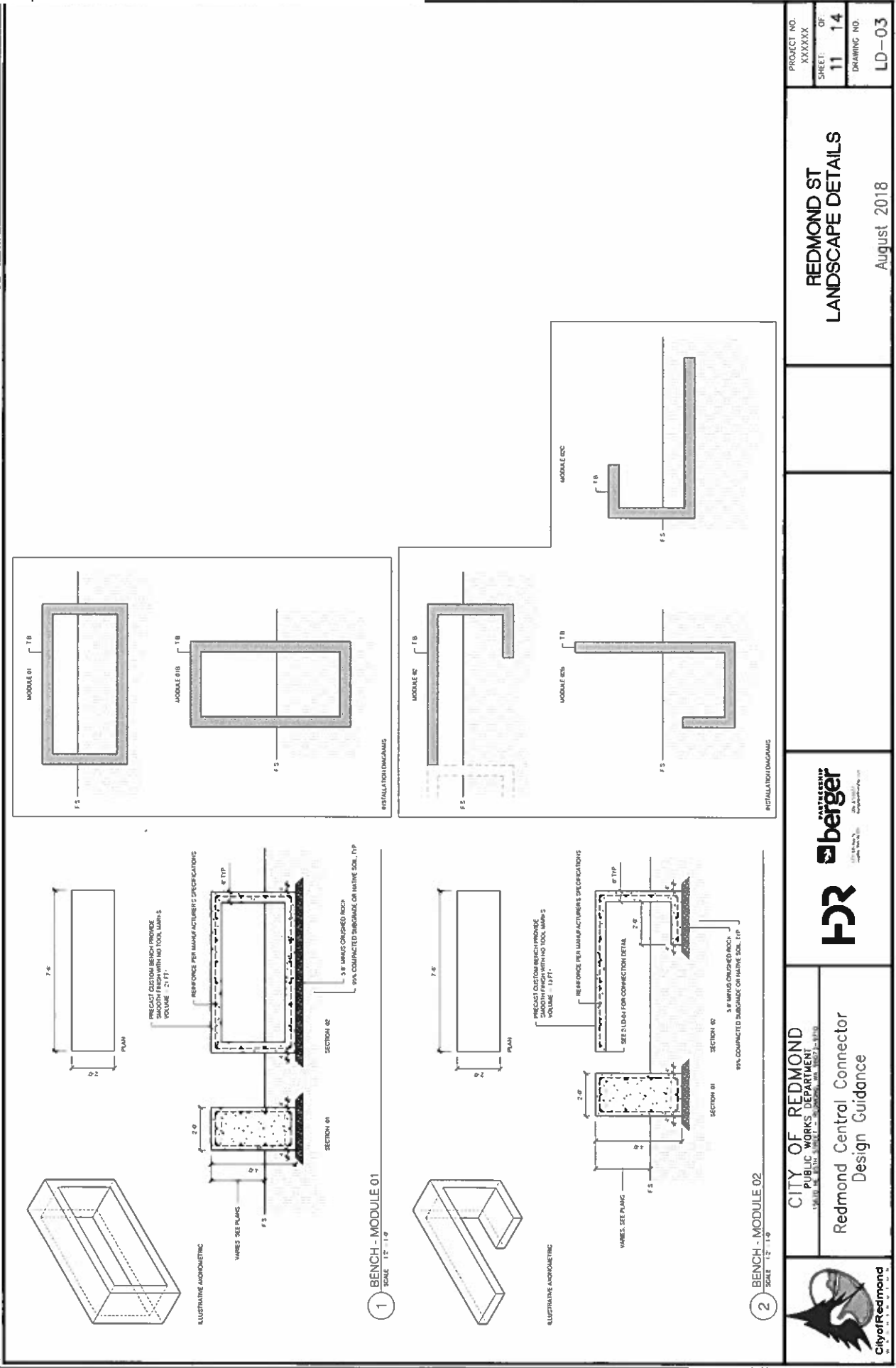
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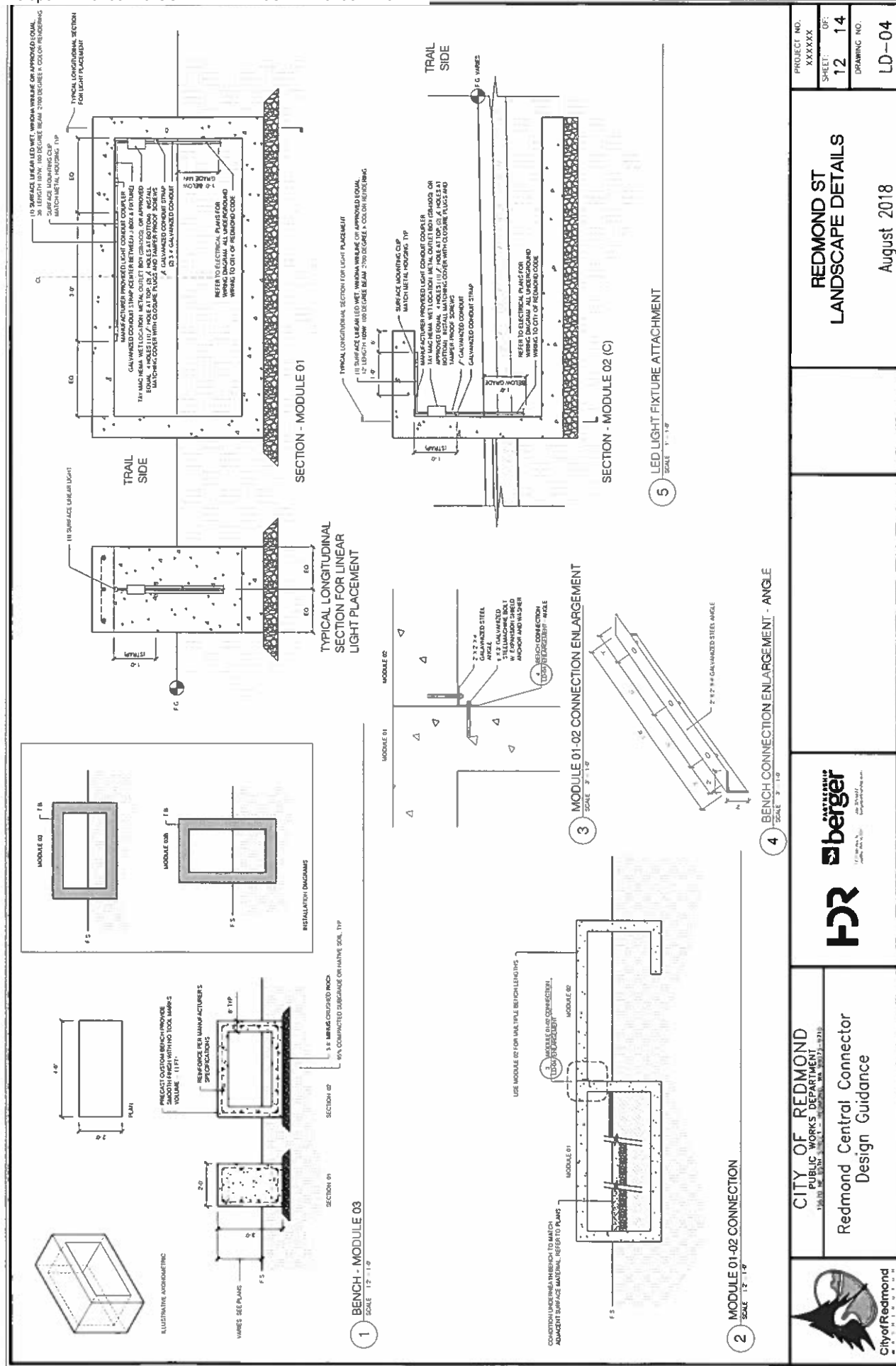
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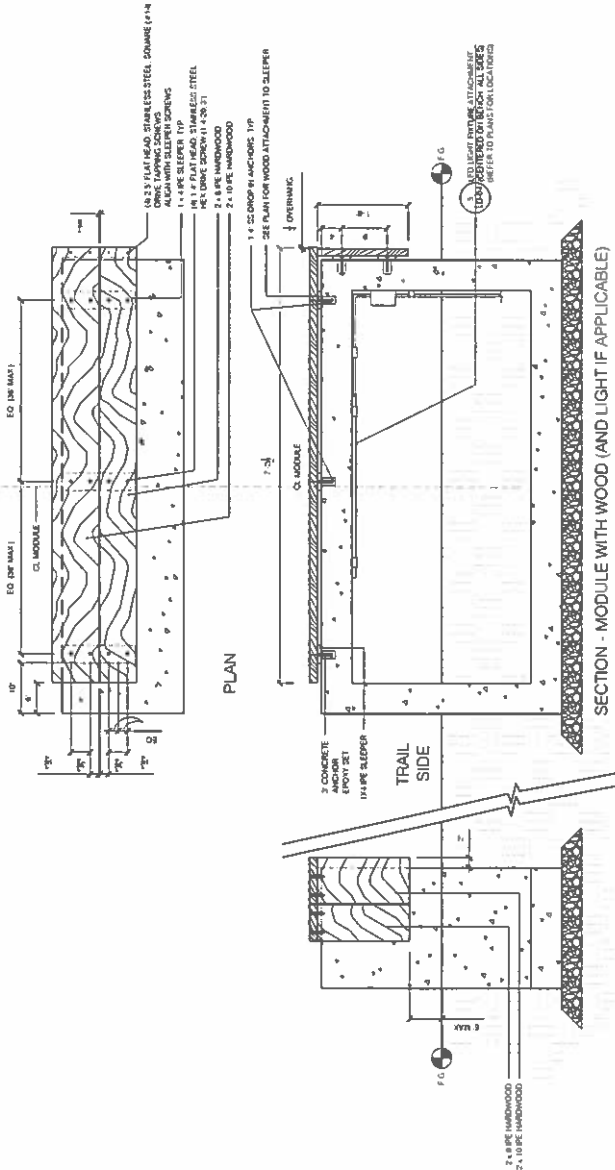
	CITY OF REDMOND PUBLIC WORKS DEPARTMENT 15600 N. MILITARY AVENUE, SUITE 100, REDMOND, OR 97053-3218	 H2R	 Haberberger LANDSCAPE ARCHITECTS 1122 NW 10TH AVE, SUITE 200 PORTLAND, OR 97209	REDMOND ST LANDSCAPE DETAILS August 2018	PROJECT NO. XXXXXX
					SHEET 10 OF 14
					DRAWING NO. LD-02





BENCH AND LIGHTING NOTES

- 1. SLOTT SHOT DRAWINGS FOR BENCH AND LIGHTING DETAILS SHALL BE PROVIDED BY THE MANUFACTURER OF THE BENCH AND LIGHTING. PRIOR TO COMPLETE INSTALLATION, SHOP DRAWINGS SHALL SHOW FINAL LAYOUT OF ALL BENCH APPLICATIONS, LIGHTING APPLICATIONS, AND BENCH WRAP APPLICATIONS WITH DIMENSIONS AND MATERIALS. THE MANUFACTURER SHALL SUBMIT MATERIAL APPROVAL FOR ALL SHOP DRAWINGS FOR REVIEW AND APPROVAL BY THE ARCHITECT.
- 2. PRE-WOOD ELEMENTS AND FSC CERTIFICATION
- 3. LIGHT FIXTURE
- 4. ELECTRICAL BOXES ABOVE GRADE
- 5. ALL FASTENERS AND HARDWARE (LIGHTING AND WOOD) SHALL BE STAINLESS STEEL OR ALUMINUM. ALL HARDWARE SHALL BE FSC CERTIFIED WITH PROPER CHAIN-OF-HANDLING SUBMITTAL PROVIDED.
- 6. END GRAIN AND BOARD JOINTS SHALL BE DONE TO REDUCE VISUAL IMPACT OF WARPING (FACE DOWN)
- 7. ALL HARDWARE TO BE STAINLESS STEEL, SQUARE DRIVE OR ALUMINUM. FASTENING TO BE PRE-DRILLED AND COUNTERSINK FLUSH
- 8. SEAL AND WAX END OF GRAIN OF PINE WITH NATURAL WAX PER MANUFACTURER'S RECOMMENDATIONS WITHIN 4 HOURS OF CUT
- 9. ALL WOOD SURFACES SHALL BE Sanded Smooth TO REMOVE ROUGH SURFACES, CLEAR OF SPLINTERS, CRACKS, OR DEFORMATIONS IN WOOD
- 10. ALL WOOD NOT DIMENSIONED ON PLAN ARE NOMINAL IN SIZE. ALL WOOD SHALL BE FSC CERTIFIED WITH PROPER CHAIN-OF-HANDLING SUBMITTAL PROVIDED
- 11. STANDARD PROJECT SKETCHES SHALL BE USED, NO SPECIAL SKETCHES FOR THE WOOD BENCH. INSTALL PER DETAIL. ALL DIMENSIONS SHALL BE DETERMINED BY LANDSCAPE ARCHITECT PER SPECIFICATIONS



1 WOOD BENCH CLADDING  
SCALE: 1" = 1'-0"



CITY OF REDMOND  
PUBLIC WORKS DEPARTMENT  
15010 N. 10TH STREET, SUITE 200, REDMOND, OR 97053-3710  
Redmond Central Connector  
Design Guidance

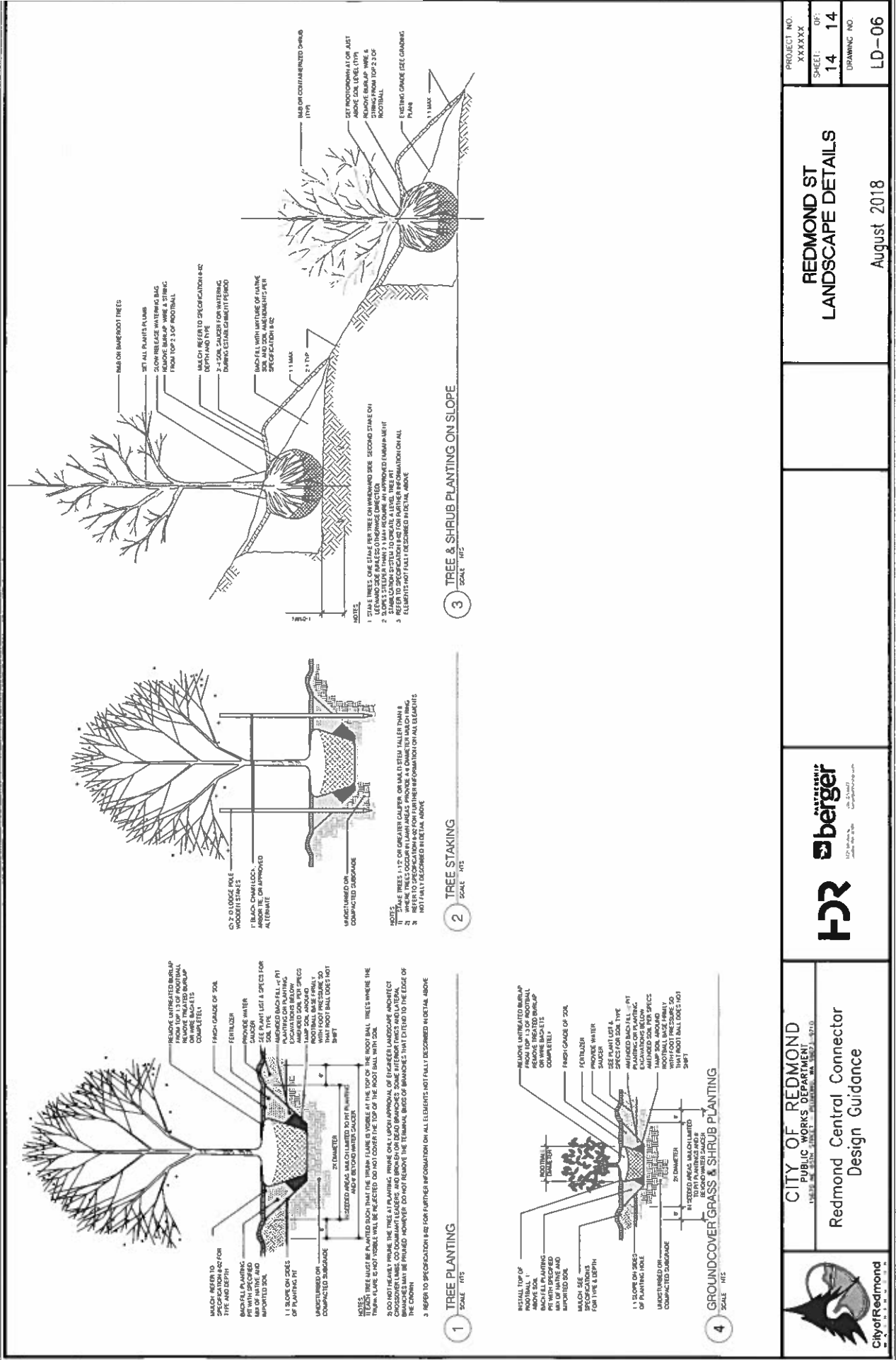


REDMOND ST  
LANDSCAPE DETAILS

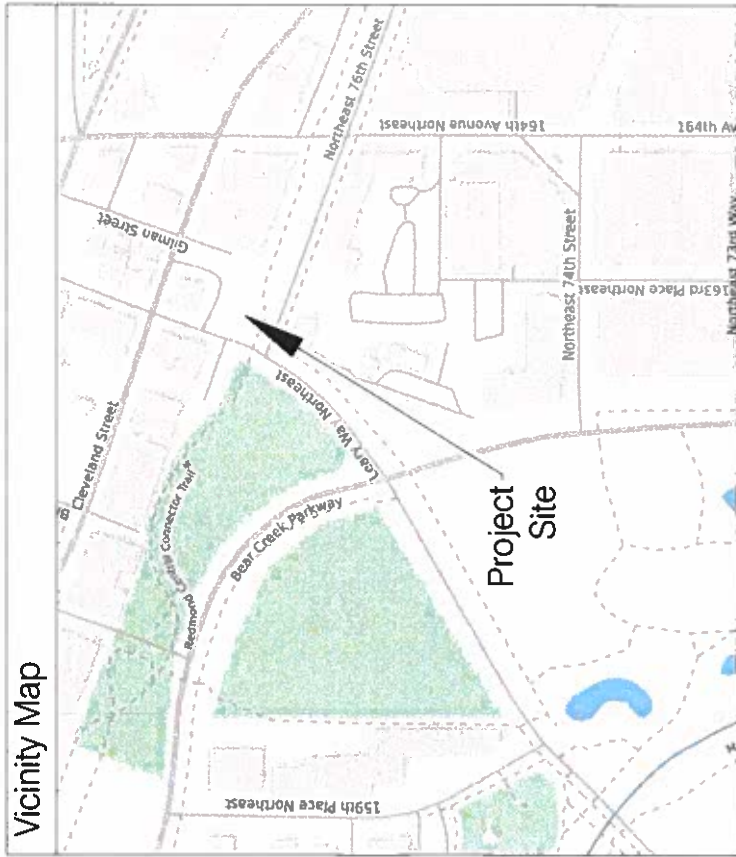
August 2018

PROJECT NO.: XXXXXX  
SHEET: 13 OF 14  
DRAWING NO.: LD-05





	<b>CITY OF REDMOND</b> PUBLIC WORKS DEPARTMENT 1500 N. REDMOND AVENUE, SUITE 100 REDMOND, OR 97001-3210	<b>FOR</b> 	<b>REDMOND ST</b> <b>LANDSCAPE DETAILS</b>  August 2018	PROJECT NO. XXXXXX
				SHEET: 14 14
				DRAWING NO. LD-06



Property Information	
Tax Parcel Number	112505-9146
Legal Description	PCL 1 REDMOND BLA # L080129 REC # 20080807900001 SD BLA BEING LOTS 1 THRU 4 BLOCK 1 TOWN OF REDMOND ADD TGM POR B/N FORMERLY NP RR R/W OVER E 1/2 OF NE 1/4 OF NE 1/4 STR 11-25-05

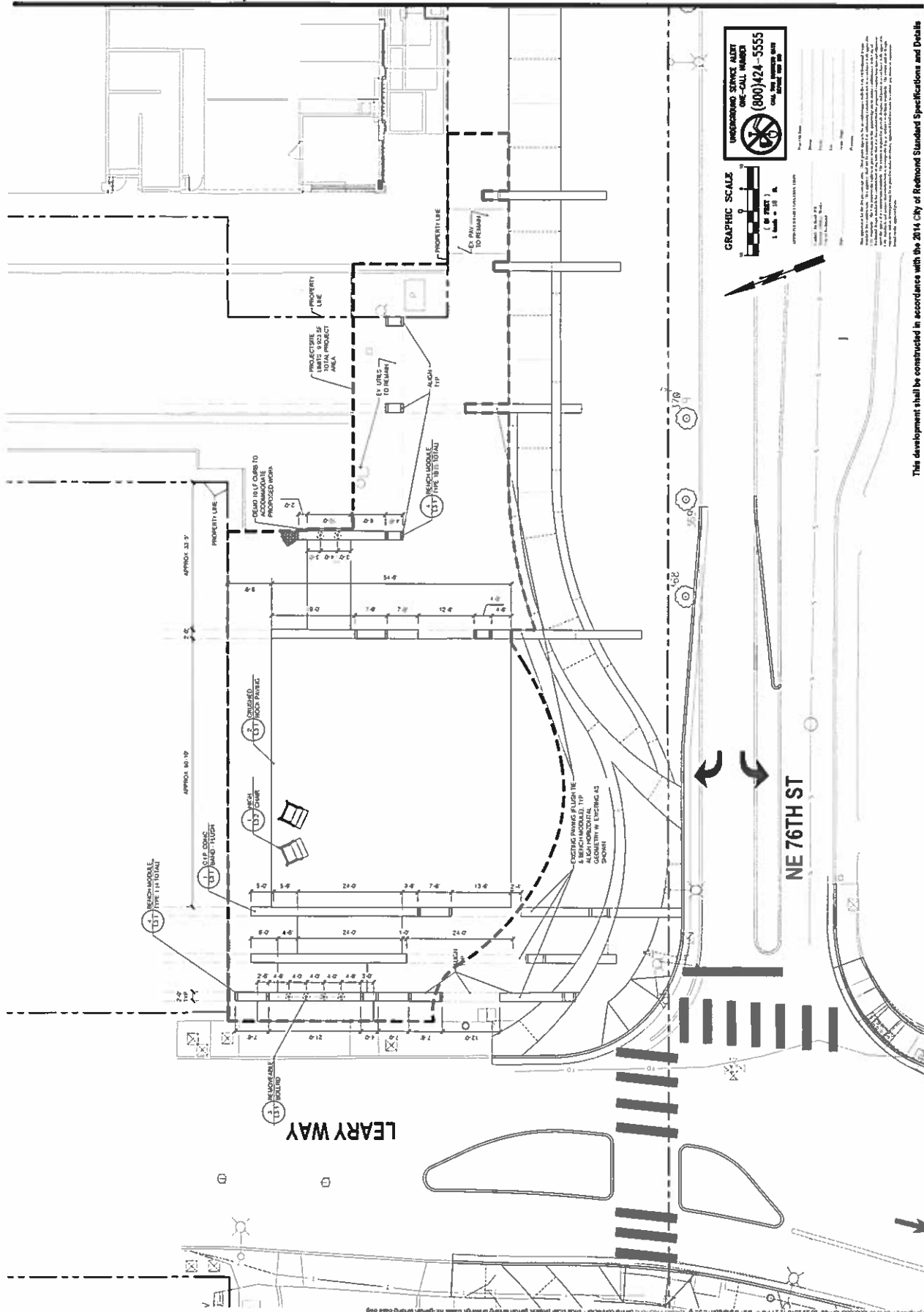
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Sheet Number	Sheet Title
L0.0	COVER SHEET
L1.0	SITE PLAN
L2.0	GRADING PLAN
L3.1	DETAILS
L3.2	DETAILS
L4.0	PLANTING PLAN
L5.5	IRRIGATION PLAN
C1.0	STORMWATER POLLUTION PREVENTION PLAN

## Contacts

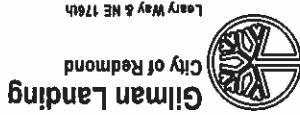
Name	Role	Organization	Phone	Email
Guy Michaelson	Landscape Architect	Berger Partnership	(206) 325-6877	guy@bergerpartnership.com
Matt Martenson	Landscape Architect Project Manager	Berger Partnership	(206) 325-6877	matm@bergerpartnership.com
Dave Tucheck	Park Operations Manager / Owner Construction Contact	City of Redmond	(425) 556-2318	dtucheck@redmond.gov
Laure Plarr	Civil Engineer	LPD Engineering	(206) 725-1211	info@lpdengineering.com

NOT INCLUDED  
THIS DRAFT SET





**This development shall be constructed in accordance with the 2014 City of Redmond Standard Specifications and Details**

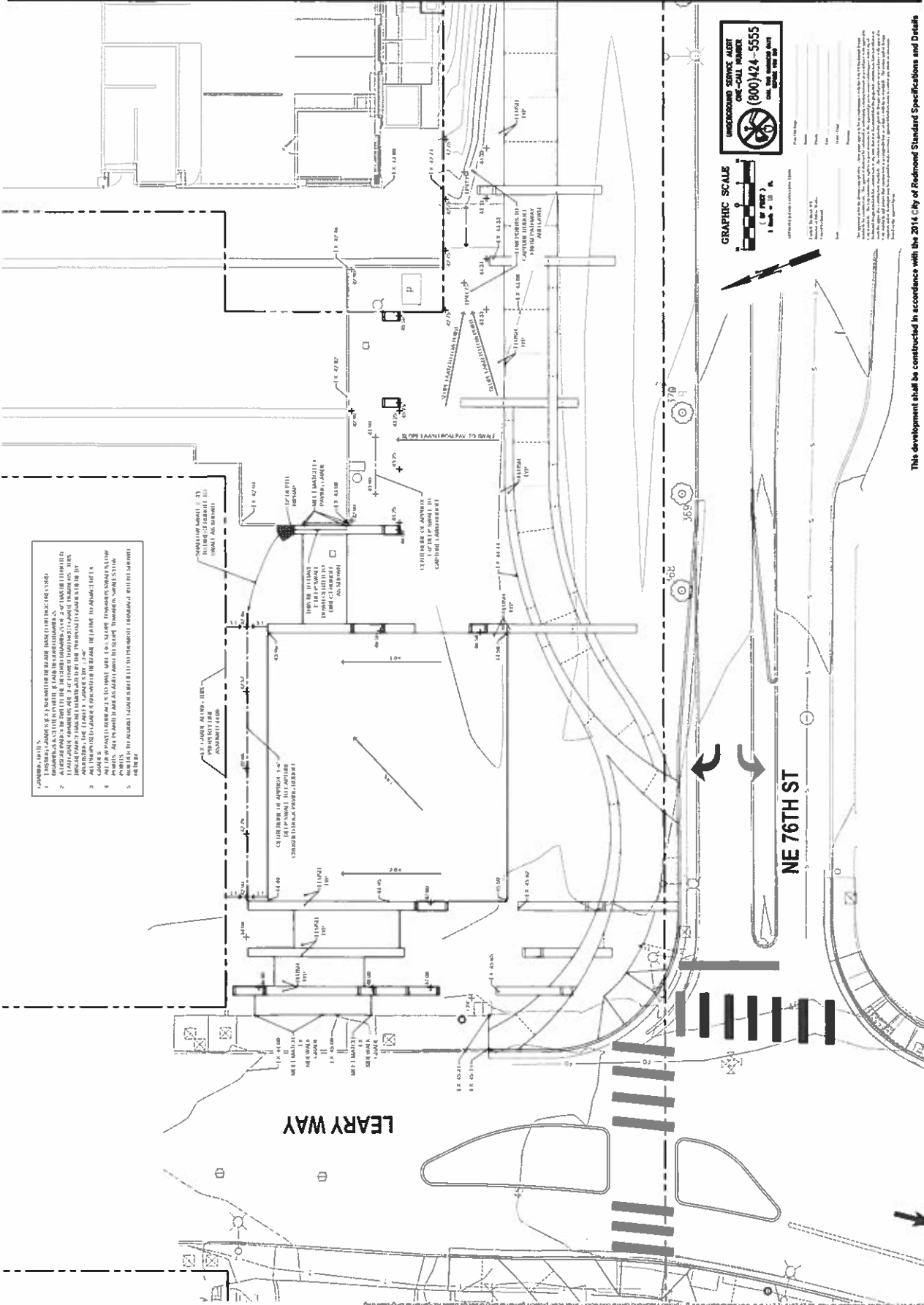


Leary Way & NE 76th

STATE OF  
WASHINGTON  
COUNTY OF  
REDMOND  
EXPIRES 06/30/2021

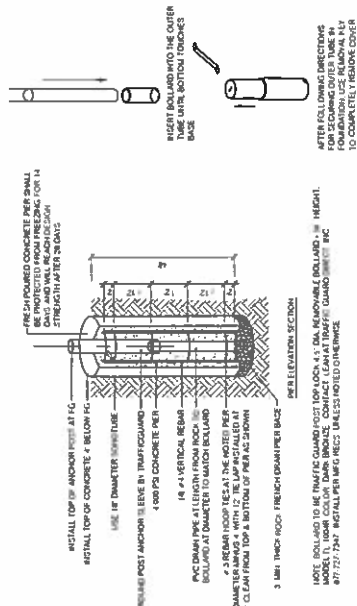
Project  
Permit  
#252016  
06/30/2021

GRADING PLAN  
L2.0

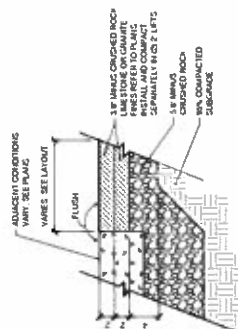


- NOTES:
1. ALL ELEVATIONS ARE IN FEET AND DECIMALS THEREOF.
  2. ALL ELEVATIONS ARE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
  3. ALL ELEVATIONS ARE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
  4. ALL ELEVATIONS ARE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
  5. ALL ELEVATIONS ARE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.

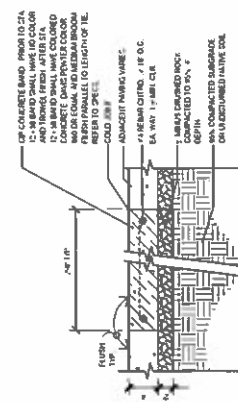
This development shall be constructed in accordance with the 2014 City of Redmond Standard Specifications and Details



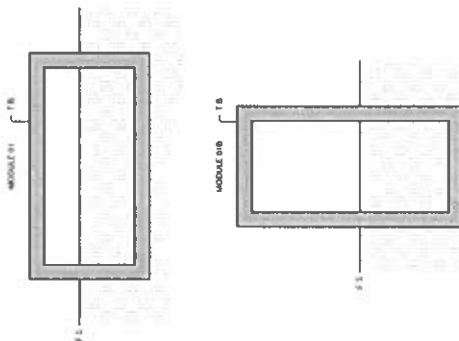
**REMOVEABLE BOLLARD**



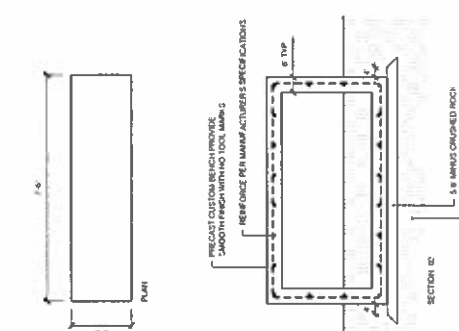
CRUSHED ROCK PAVING



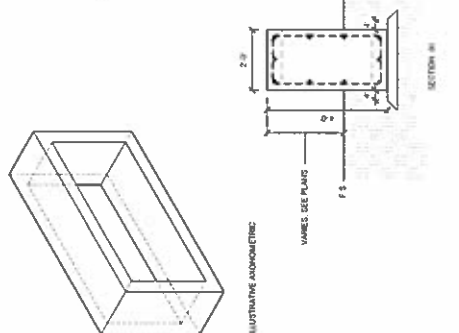
C.I.P. CONC. BAND - FLUSH



## INSTALLATION DIAGRAMS



5.8 MM CRUSHED ROCK



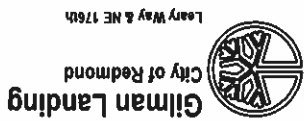
ILLUSTRATIVE AXIOMATIZATION;

**NOTE**  
FOR FABRICATION AND COST CONTACT OLYMPIAN  
PRECAST, PHONE NUMBER 428 684 1822

BENCH MODULE TYPE 1

**This development shall be constructed in accordance with the 2014 City of Redmond Standard Specifications and Details**





City of Redmond  
3250 NE 17th Ave  
Redmond, OR 97756  
(503) 922-1100

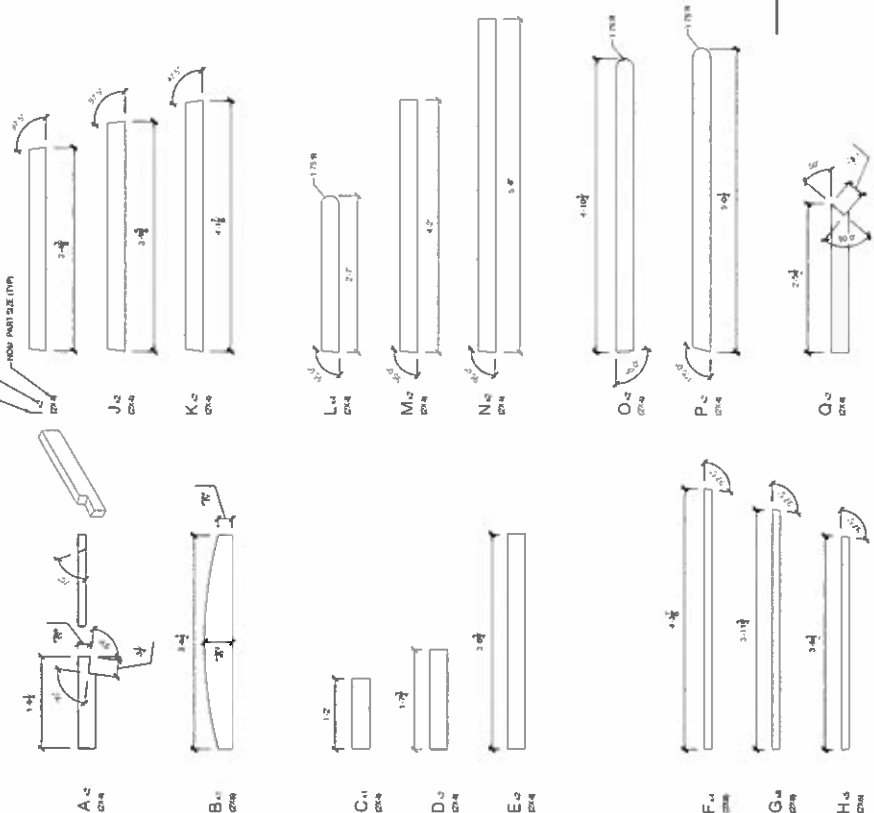
Permit  
32502016

DETAILS

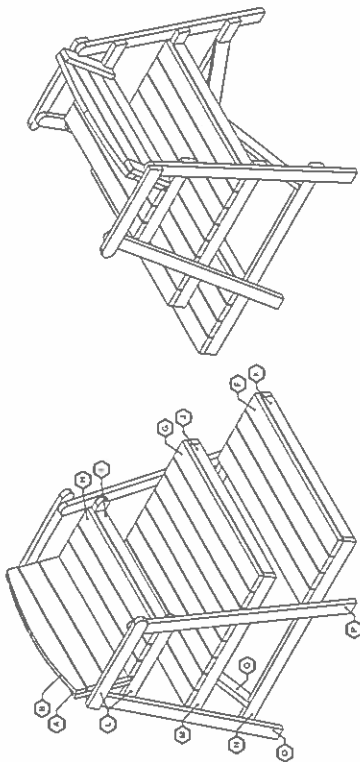
L3.2

- NOTES:
- 1. 2X4 NOM. LACTUM 3.5' x 1.5' x 8'
  - 2. 2X4 NOM. LACTUM 3.5' x 1.5' x 5'
  - 3. WOOD SPECIES AND GRADE TBD
  - 4. FASTENERS SHALL BE GALVANNEAL
  - 5. FASTENERS SHALL BE GALVANNEAL

PART LIST

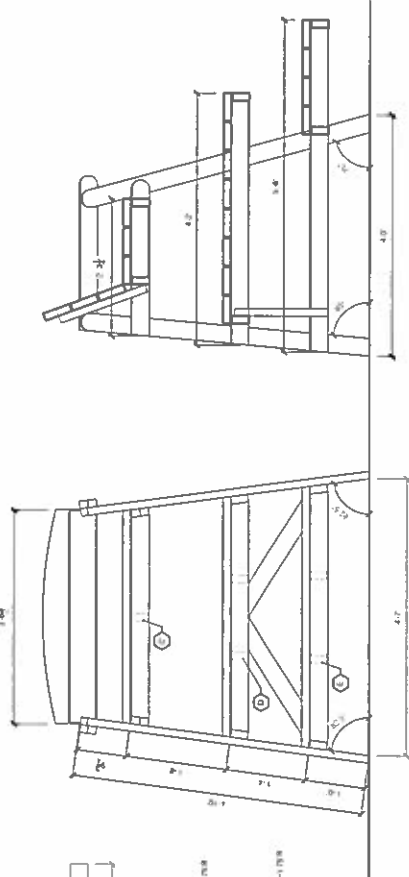


1 HIGH CHAIR  
Scale: 1/4" = 1'-0"



ISO-FRONT

ISO-BACK



FRONT ELEV.

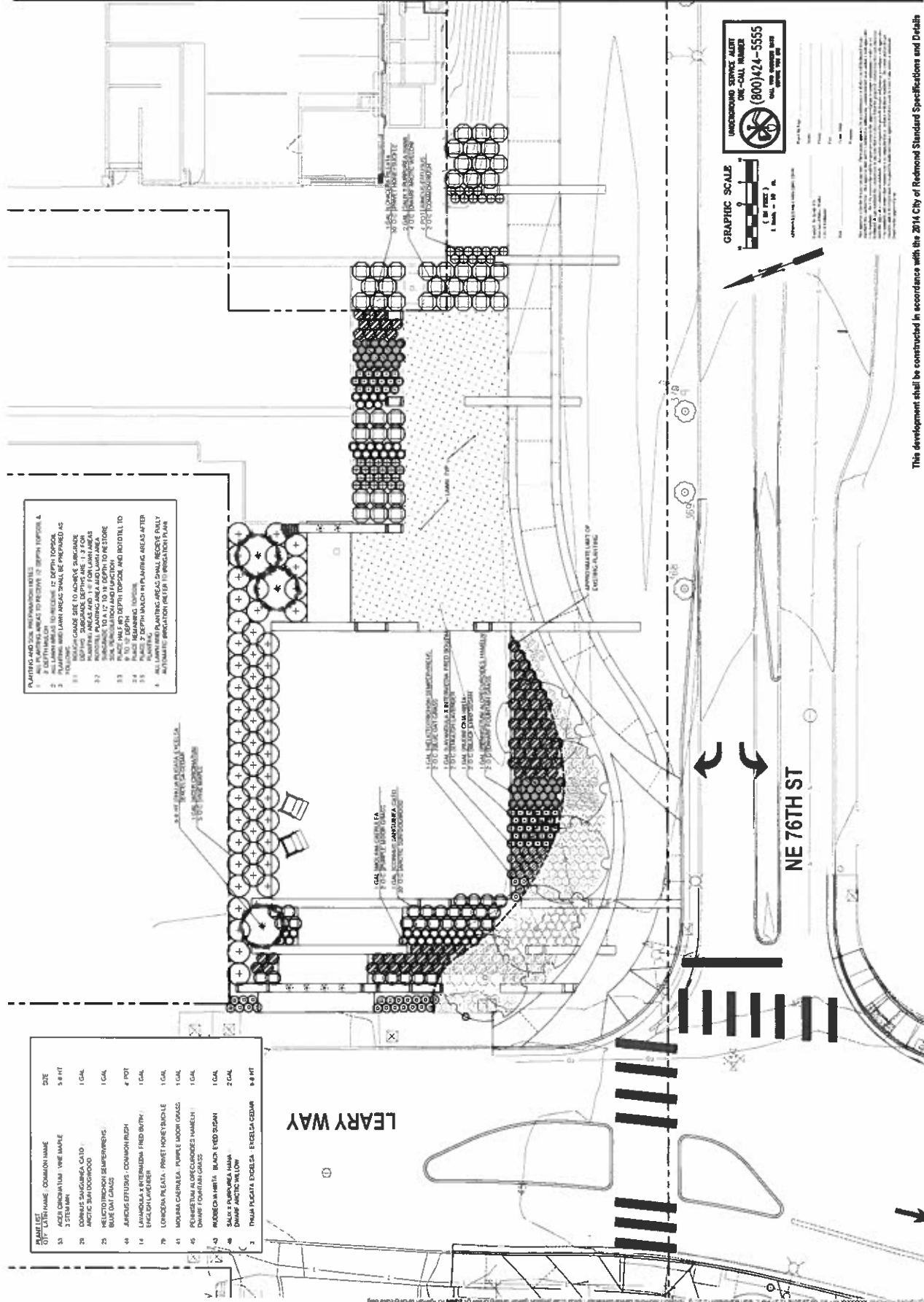
SECTION

GRAPHIC SCALE  
(in feet)  
1" = 1'-0"

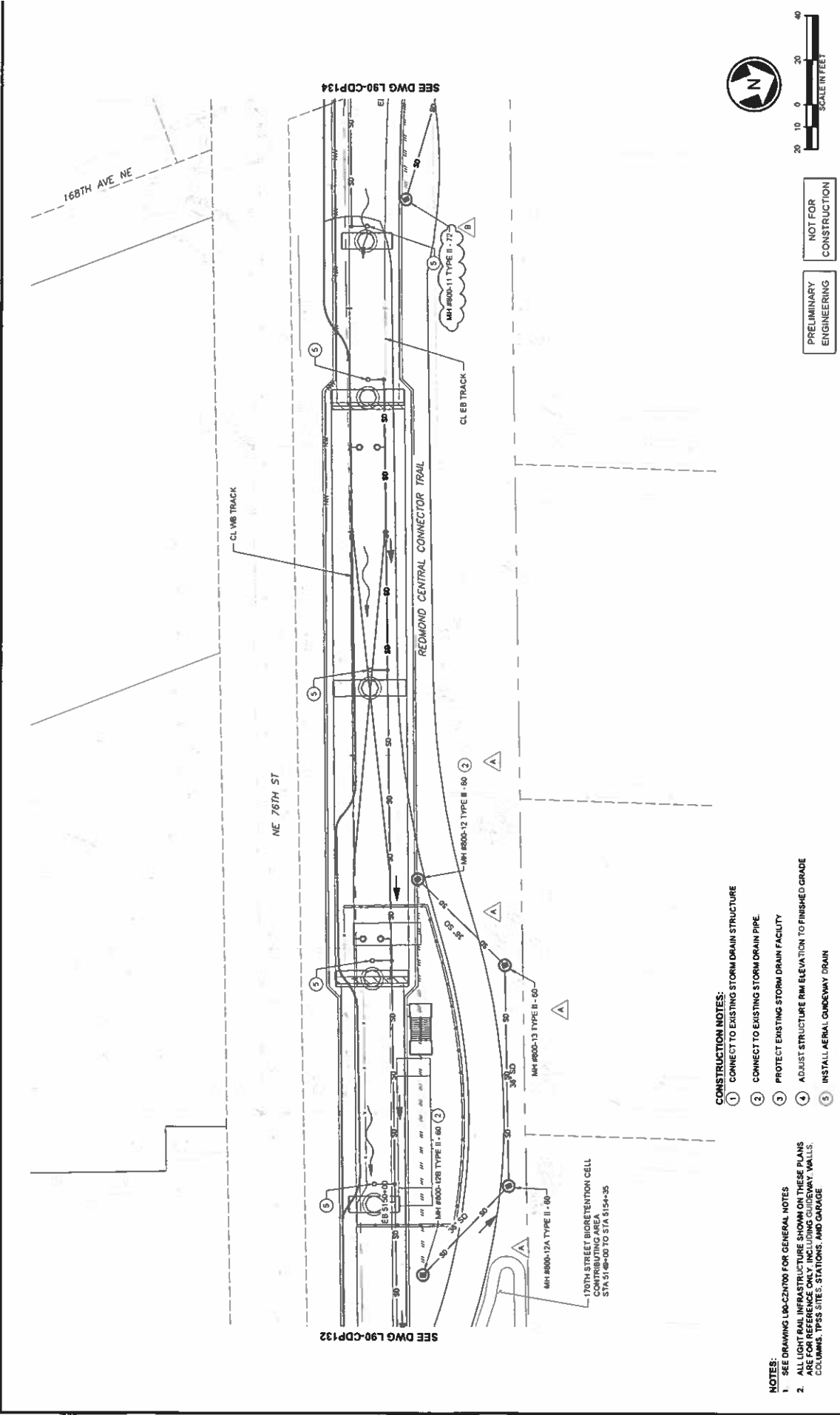


UNDERGROUND SERVICE ALERT  
ONE-CALL NUMBER  
(800) 424-5555  
CALL NOW TO LOCATE  
ALL UTILITIES BEFORE YOU DIG

This development shall be constructed in accordance with the 2014 City of Redmond Standard Specifications and Details



**This development shall be constructed in accordance with the 2014 City of Redmond Standard Specifications and Details**

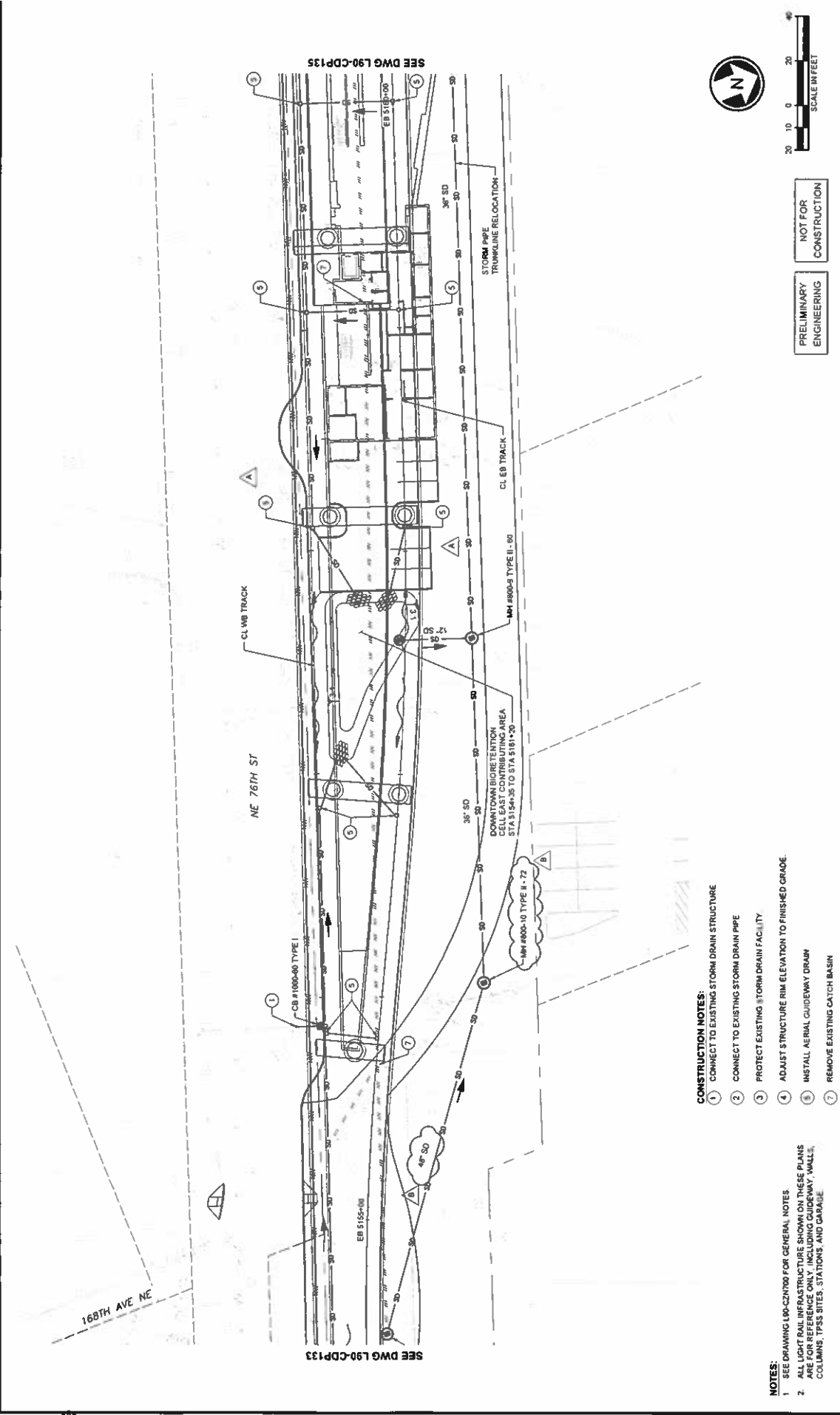


**NOTES:**  
1. SEE DRAWING L90-CDP133 FOR GENERAL NOTES  
2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE

- CONSTRUCTION NOTES:**
- 1. CONNECT TO EXISTING STORM DRAIN STRUCTURE
  - 2. CONNECT TO EXISTING STORM DRAIN PIPE
  - 3. PROTECT EXISTING STORM DRAIN FACILITY
  - 4. ADJUST STRUCTURE RM ELEVATION TO FINISHED GRADE
  - 5. INSTALL AERIAL GUIDEWAY DRAIN

<b>CONCEPTUAL DESIGN</b>		<b>REVISIONS</b>		<b>APPROVED BY</b>		<b>DATE</b>	
A	02/07/19	MMS	BFP	ADDERKUM 10 - REVISION FOR TPSS #3	J. TYLER	02/24/2018	
B	02/08/19	MMS	BFP	ADDERKUM 11 - STORM SIZES REVISION	J. HITCH	02/24/2018	
		DATE	CHK	APP	REVISION		
				<b>DESIGNED BY</b>		<b>DATE</b>	
				T. NABOURS		02/24/2018	
				<b>CHECKED BY</b>		<b>DATE</b>	
				J. TYLER		02/24/2018	
				<b>APPROVED BY</b>		<b>DATE</b>	
				V. YANG		02/24/2018	
				<b>PROJECT</b>		<b>DATE</b>	
				DOWNTOWN REDMOND LINK EXTENSION		02/24/2018	
				<b>CLIENT</b>		<b>DATE</b>	
				RTS TO DOWNTOWN REDMOND		02/24/2018	
				<b>SCALE</b>		<b>DATE</b>	
				1" = 20'		02/24/2018	
				<b>PROJECT NO.</b>		<b>DATE</b>	
				L90-CDP133		02/24/2018	
				<b>PROJECT NAME</b>		<b>DATE</b>	
				DRAINAGE PLAN		02/24/2018	
				<b>PROJECT NO.</b>		<b>DATE</b>	
				209		02/24/2018	
				<b>PROJECT NO.</b>		<b>DATE</b>	
				209		02/24/2018	
				<b>PROJECT NO.</b>		<b>DATE</b>	
				209		02/24/2018	

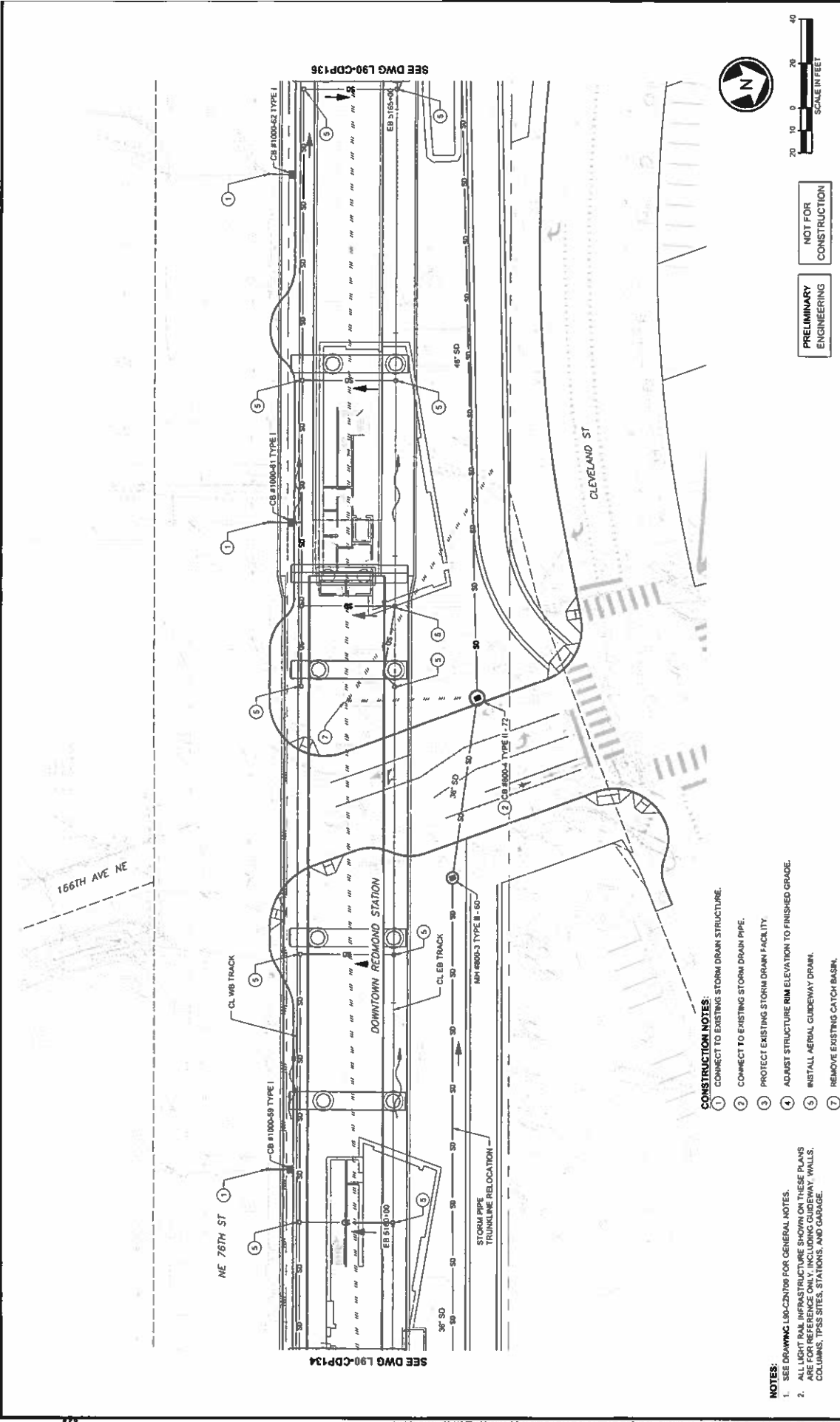




- CONSTRUCTION NOTES:**
- 1. CONNECT TO EXISTING STORM DRAIN STRUCTURE
  - 2. CONNECT TO EXISTING STORM DRAIN PIPE
  - 3. PROTECT EXISTING STORM DRAIN FACILITY
  - 4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE
  - 5. INSTALL AERIAL GUIDEWAY DRAIN
  - 6. REMOVE EXISTING CATCH BASIN

- NOTES:**
- 1. SEE DRAWING L90-C2000 FOR GENERAL NOTES.
  - 2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

CONCEPTUAL DESIGN				DOWNTOWN REDMOND LINK EXTENSION RTS TO DOWNTOWN REDMOND				L90-CDP134											
NO.	DATE	BY	CHK.	APP.	REVISION	NO.	DATE	BY	CHK.	APP.	REVISION	NO.	DATE	BY	CHK.	APP.	REVISION		
A	02/01/19	JMS	BFP	BFP	ADDENDUM #5 - REVISION FOR TPSS #3														
B	03/08/19	JMS	BFP	BFP	ADDENDUM #11 - STORM SIZES REVISION														
DESIGNED BY				CHECKED BY				APPROVED BY				SUBMITTED BY				DATE			
T. NABOURS				J. TYLER				J. HITCH				BRAD PHILLIPS				08/24/2018			
PARAMETRIX				PARAMETRIX				PARAMETRIX				PARAMETRIX				PARAMETRIX			
SCALE				FILE NAME				PROJECT NO.				LOCATION				SHEET NO.			
1" = 20'				L90-CDP134.DWG				L90-CDP134				DOWNTOWN REDMOND				210			
DATE				DATE				DATE				DATE				DATE			
08/24/2018				08/24/2018				08/24/2018				08/24/2018				08/24/2018			



**NOTES:**  
1. SEE DRAWING L90-CDP135 FOR GENERAL NOTES.  
2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

- CONSTRUCTION NOTES:**
- 1. CONNECT TO EXISTING STORM DRAIN STRUCTURE.
  - 2. CONNECT TO EXISTING STORM DRAIN PIPE.
  - 3. PROTECT EXISTING STORM DRAIN FACILITY.
  - 4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE.
  - 5. INSTALL AERIAL GUIDEWAY DRAIN.
  - 7. REMOVE EXISTING CATCH BASIN.

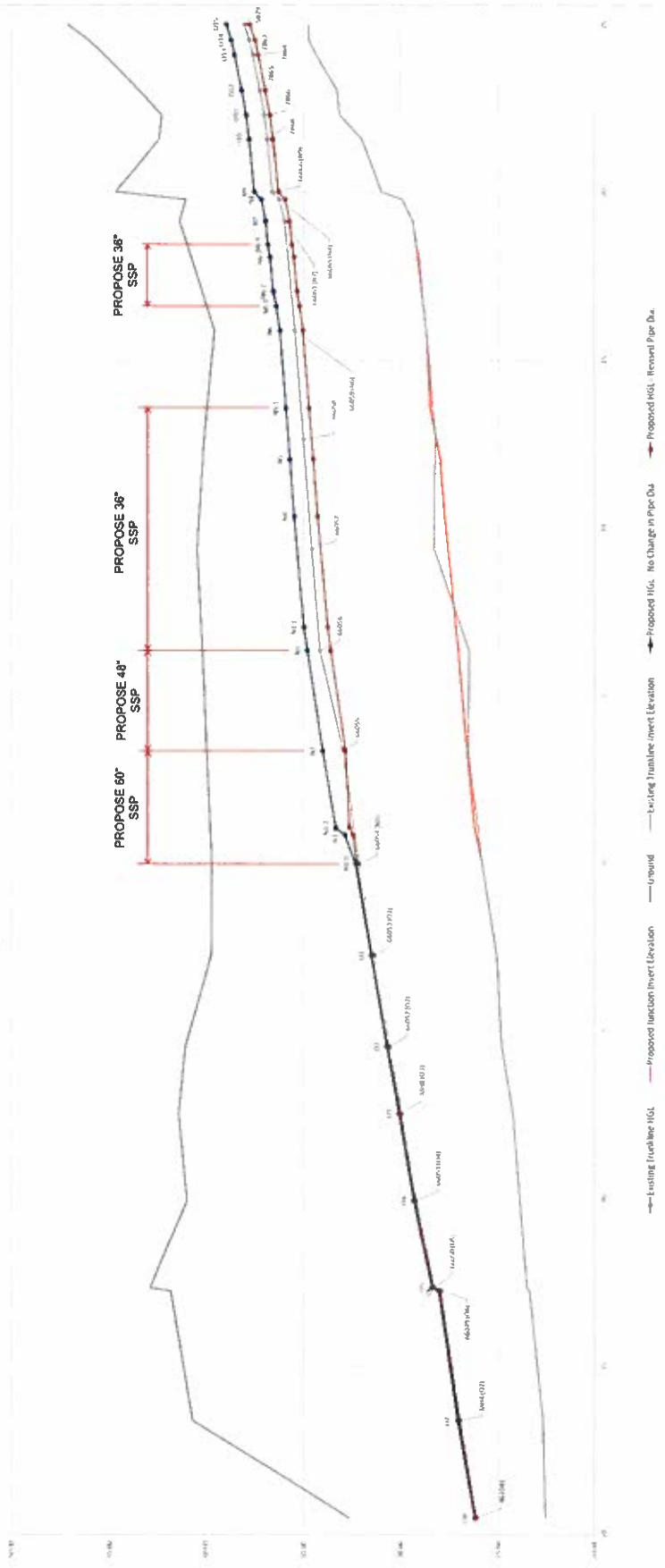
PRELIMINARY  
ENGINEERING

NOT FOR  
CONSTRUCTION

<b>CONCEPTUAL DESIGN</b>		<b>Parametrix</b> Engineering & Construction Services		<b>Sound Transit</b>		<b>DOWNTOWN REDMOND LINK EXTENSION RTS TO DOWNTOWN REDMOND</b>		<b>L90-CDP135</b>	
DATE	10/19/18	DESIGNED BY	T. ANDERSON	SCALE	1" = 20'	PROJECT NAME	RTS TO DOWNTOWN REDMOND	LOCATION	REDMOND, WA
DATE	10/19/18	DRAWN BY	J. TYLER	CONTRACT NO.	CH 11-18-18	CIVIL	DRAINAGE PLAN	SHEET NO.	211
DATE	10/19/18	CHECKED BY	J. HITCH	DATE	09/24/2018	DATE	09/24/2018	DATE	09/24/2018
DATE	10/19/18	APPROVED BY	Y. YANG	DATE	09/24/2018	DATE	09/24/2018	DATE	09/24/2018

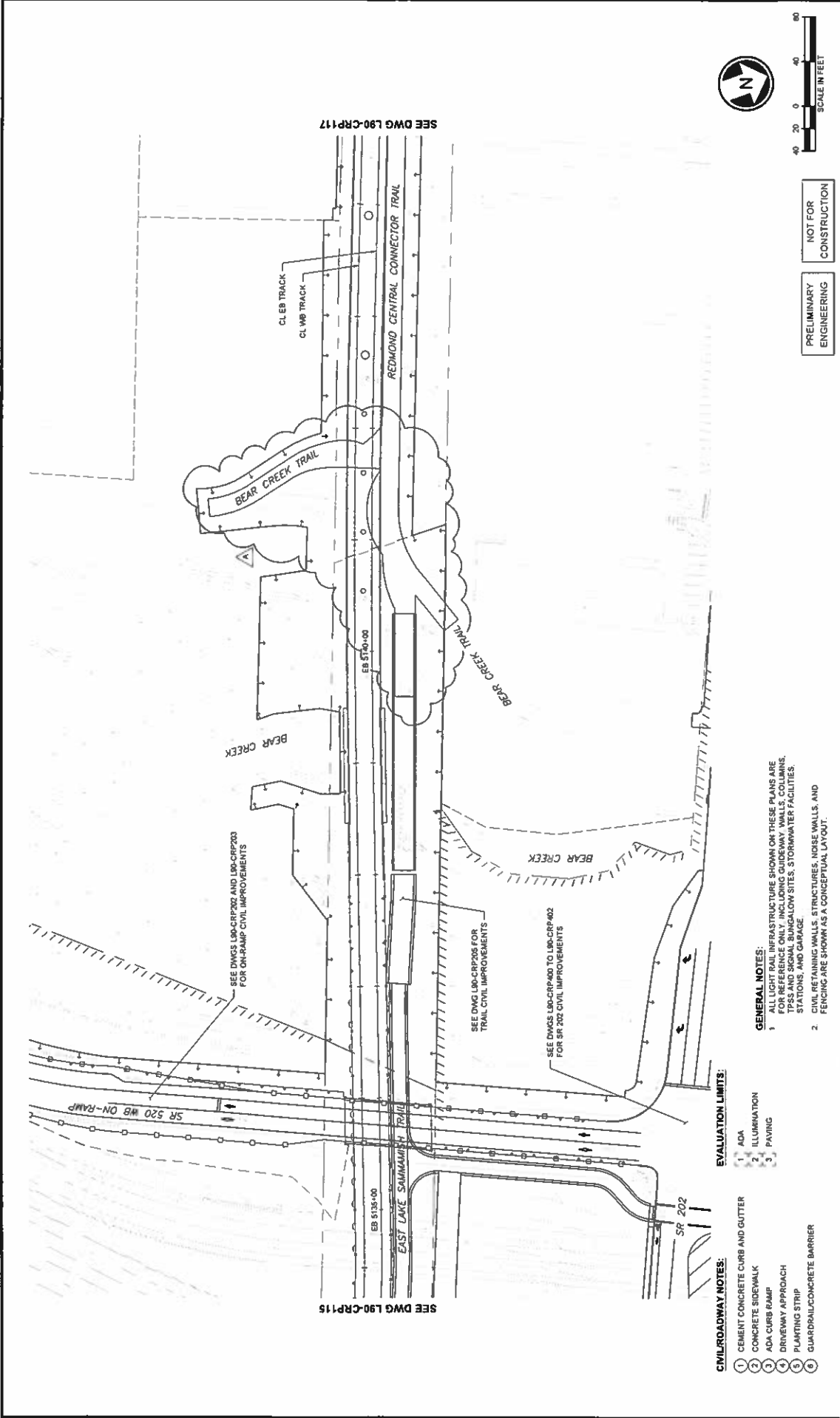


DRIE - City of Redmond Downtown Trunkline Relocation Model Results



NOT TO SCALE

Figure 2. Hydraulic Grade Line of Existing and Proposed Trunkline



PRELIMINARY  
ENGINEERING

NOT FOR  
CONSTRUCTION



- CIVIL/ROADWAY NOTES:**
- 1 CEMENT CONCRETE CURB AND GUTTER
  - 2 CONCRETE SIDEWALK
  - 3 ADA CURB RAMP
  - 4 DRIVEWAY APPROACH
  - 5 PLANTING STRIP
  - 6 GUARDRAIL/CONCRETE BARRIER
- EVALUATION LIMITS:**
- 1 ADA
  - 2 ILLUMINATION
  - 3 PAVING
- GENERAL NOTES:**
- 1 ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY WALLS, COLUMNS, STATIONS AND GARAGE
  - 2 CIVIL RETAINING WALLS, STRUCTURES, NOISE WALLS, AND FENCING ARE SHOWN AS A CONCEPTUAL LAYOUT.

REVISIONS

NO.	DATE	DESCRIPTION
1	02/01/19	ISSUED FOR PERMIT
2	02/01/19	REVISED FOR COMMENTS
3	02/01/19	REVISED FOR COMMENTS
4	02/01/19	REVISED FOR COMMENTS
5	02/01/19	REVISED FOR COMMENTS
6	02/01/19	REVISED FOR COMMENTS
7	02/01/19	REVISED FOR COMMENTS
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49	02/01/19	REVISED FOR COMMENTS
50	02/01/19	REVISED FOR COMMENTS

**CONCEPTUAL DESIGN**

**CONFORMED SET**

DATE: 02/01/19

DESIGNED BY: JMS

CHECKED BY: BFP

APPROVED BY: JMS

DATE: 02/01/19

DESIGNED BY: JMS

CHECKED BY: BFP

APPROVED BY: JMS

**Parametrix**

STATIONARY

PROJECT BY: M. STUMPF

CHECKED BY: D. DIMOCK

APPROVED BY: D. PROCTOR

DATE: 09/22/2018

DESIGNED BY: BRAD PHILLIPS

**Sound Transit**

SCALE: 1" = 40'

FILE: L90-CRP116.DWG

CONTRACT NO: CN 0148-18

DATE: 03/29/2019

**DOWNTOWN REDMOND LINK EXTENSION**

**RTS TO DOWNTOWN REDMOND**

**PRESCRIPTIVE**

CIVIL

CORRIDOR MODIFICATION PLAN

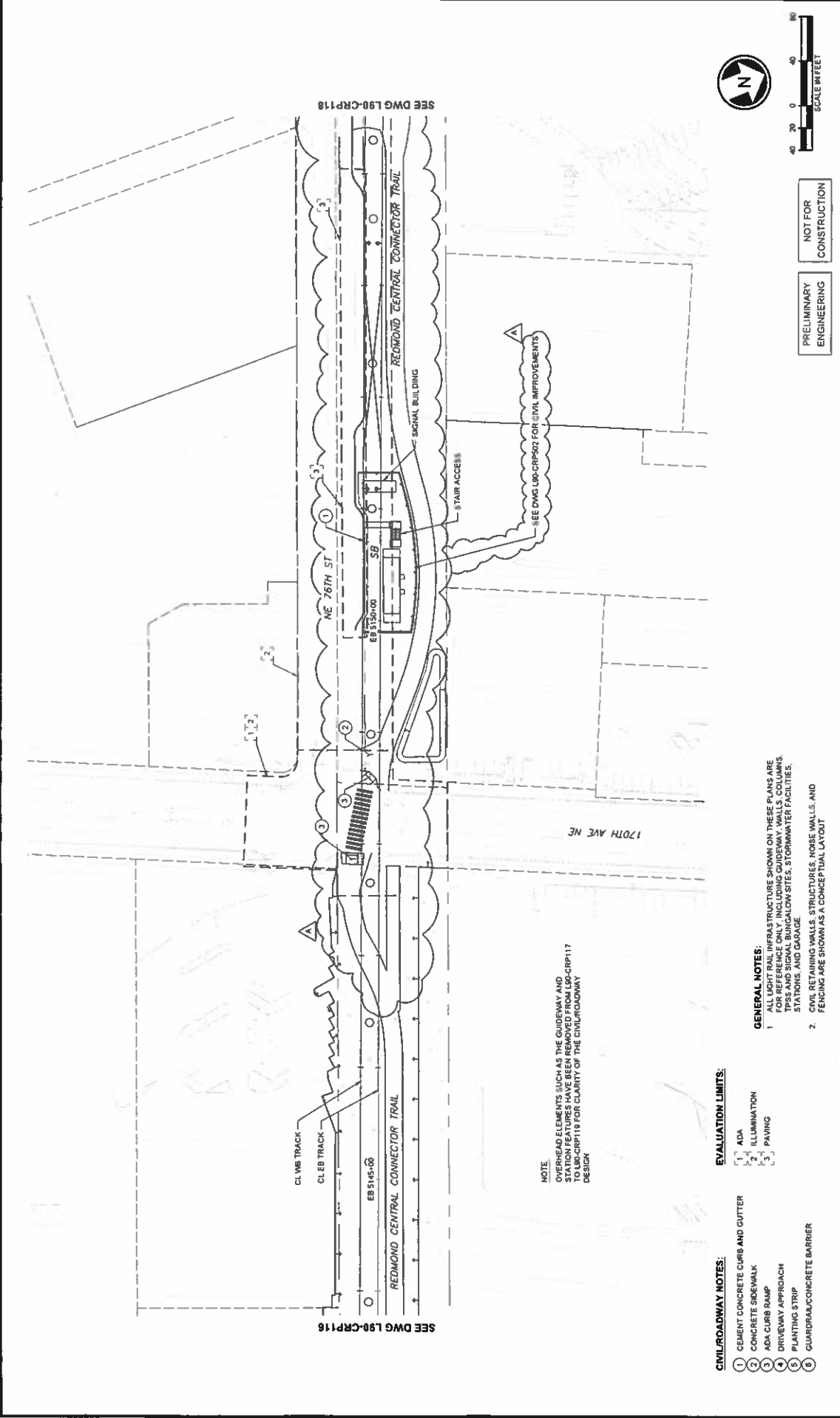
EB STA 5135+00 TO EB STA 5144+50

**L90-CRP116**

LOCATION: DOWNTOWN

SHEET NO: 156

REV: A



DATE: 03/28/2018  
DRAWN BY: [Signature]  
CHECKED BY: [Signature]  
APPROVED BY: [Signature]

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**CIVIL/ROADWAY NOTES:**

- CEMENT CONCRETE CURB AND GUTTER
- CONCRETE SIDEWALK
- ADA CURB RAMP
- DRIVEWAY APPROACH
- PLANTING STRIP
- GUARDRAIL/CONCRETE BARRIER

**EVALUATION LIMITS:**

- ADA
- ILLUMINATION
- PAVING

**GENERAL NOTES:**

- ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE PRELIMINARY. THE LOCATION OF THE TRACKS, STATIONS, AND GARAGE ARE SUBJECT TO CHANGE. THE LOCATION OF THE TRACKS, STATIONS, AND GARAGE ARE SUBJECT TO CHANGE.
- CIVIL RETAINING WALLS, STRUCTURES, NOISE WALLS, AND FENCING ARE SHOWN AS A CONCEPTUAL LAYOUT.

**CONCEPTUAL DESIGN**

**CONFORMED SET**

**ADDENDUM #6 - TIPS & REVISION**

DATE	FROM	TO	REVISION
02/01/19	MMS	BFP	REVISION





**CONFORMED SET**

**J. BETZVOG**  
CHECKED BY  
**D. PROCTOR**  
APPROVED BY  
**Y. YANG**



DATE	08/24
BY	AD PHILLIPS

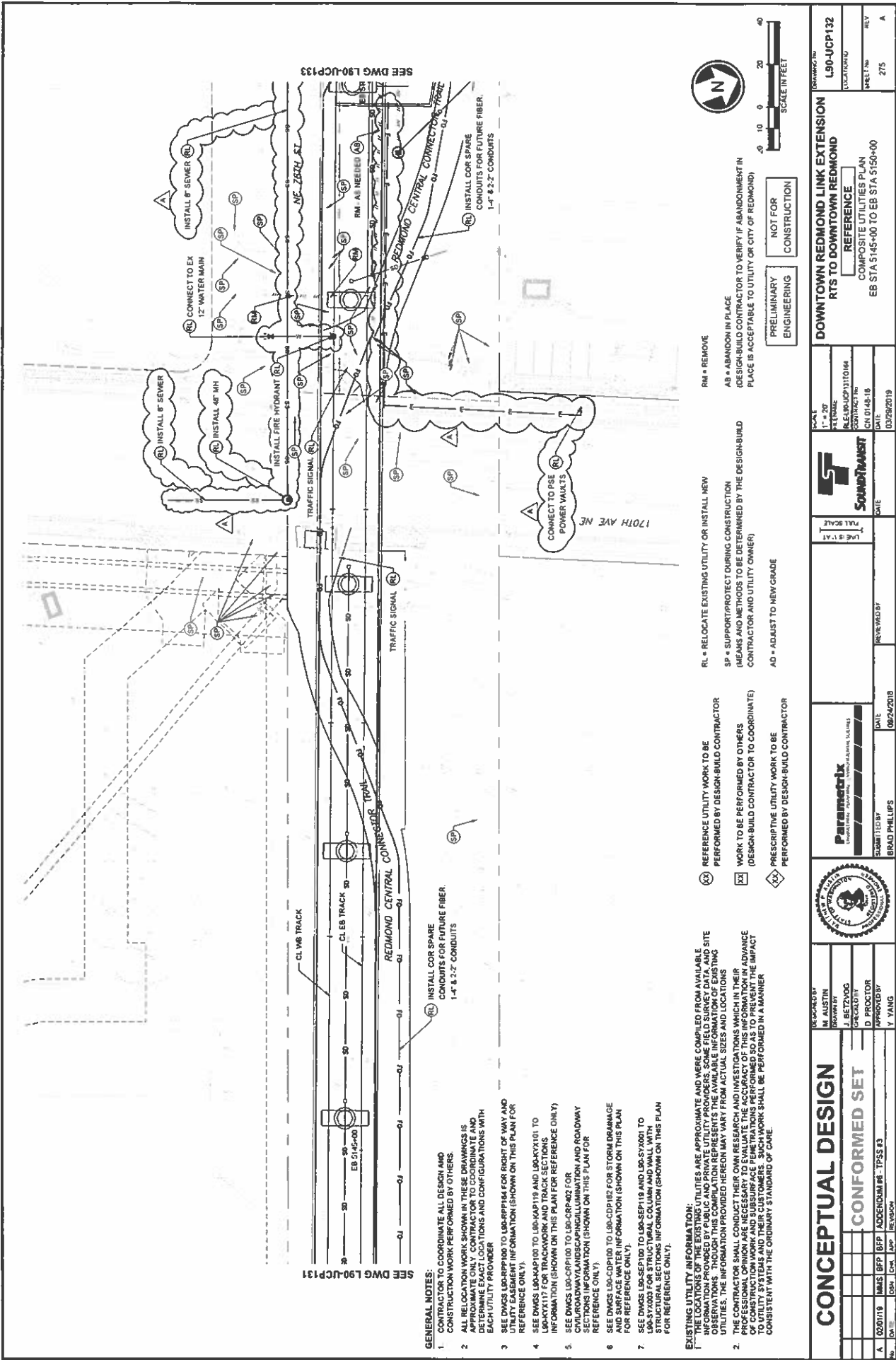
711

FILE-190-UCP13110164
CONTRACT No.
CN 014B-18
DATE
03/29/2019

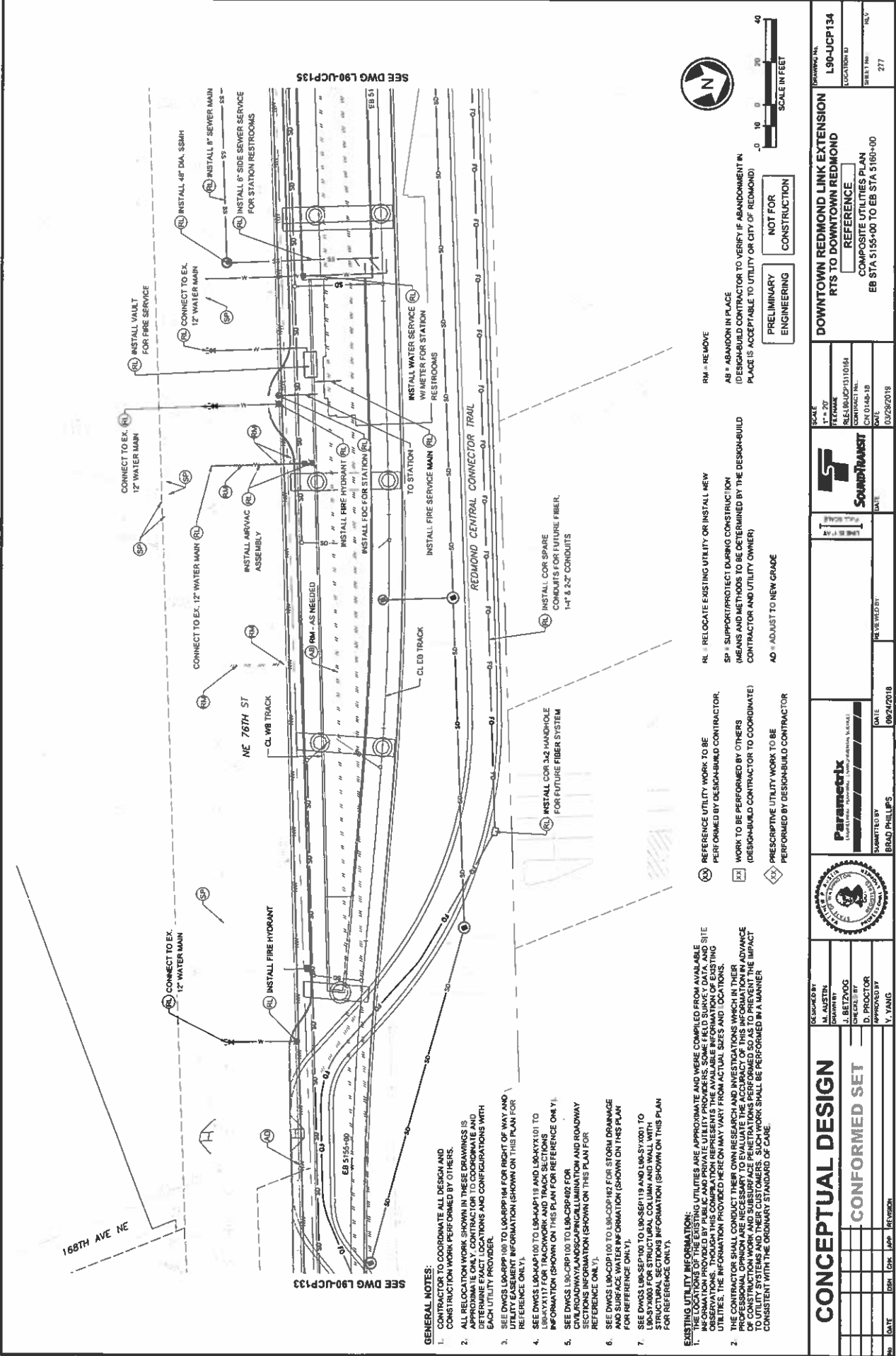
**REFERENCE**

COMPOSITE UTILITIES PLAN  
EB STA 5140+00 TO EB STA 5145+00





[illegible]



<b>CONCEPTUAL DESIGN</b>		<b>CONFORMED SET</b>	
DATE	09/24/2018	DATE	09/24/2018
DESIGNED BY	M. ADAMS	CHECKED BY	J. BETZOG
DRAWN BY	M. ADAMS	APPROVED BY	D. PROCTOR
SCALE	AS SHOWN	SCALE	AS SHOWN
PROJECT	RTS TO DOWNTOWN REDMOND	PROJECT	RTS TO DOWNTOWN REDMOND
LOCATION	EB STA 5155+00 TO EB STA 5160+00	LOCATION	EB STA 5155+00 TO EB STA 5160+00
SHEET NO.	277	SHEET NO.	277
<b>Parametrix</b> CONSULTING ENGINEERS, P.C. 10000 NE 28TH AVE, SUITE 100 REDMOND, OR 97053 TEL: 503.928.8800 WWW.PARAMETRIX.COM		<b>Sound Transit</b> 10000 NE 28TH AVE, SUITE 100 REDMOND, OR 97053 TEL: 503.928.8800 WWW.SOUNDTRANSIT.COM	
SUBMITTED BY BRAD PHILLIPS		DATE 09/24/2018	
PERFORMED BY DESIGN-BUILD CONTRACTOR		DATE 09/24/2018	
REFERENCE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR.		DATE 09/24/2018	
WORK TO BE PERFORMED BY OTHERS (DESIGN-BUILD CONTRACTOR TO COORDINATE)		DATE 09/24/2018	
PRESCRIPTIVE UTILITY WORK TO BE PERFORMED BY DESIGN-BUILD CONTRACTOR		DATE 09/24/2018	



1. CONTRACTOR TO COORDINATE ALL DESIGN AND CONSTRUCTION WORK PERFORMED BY OTHERS.
2. ALL RELOCATION WORK SHOWN IN THESE DRAWINGS IS APPROXIMATE ONLY. CONTRACTOR TO COORDINATE AND VERIFY ALL DIMENSIONS AND CONFIGURATIONS WITH EACH UTILITY PROVIDER.
3. SEE DWGS 150-SEP-100 TO 150-SEP-145 FOR RIGHT OF WAY AND UTILITY EASEMENT INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
4. SEE DWGS 150-SEP-100 TO 150-SEP-145 AND 150-XY-101 TO 150-XY-117 FOR TRACKWORK AND TRACK SECTIONS INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
5. SEE DWGS 150-CP-100 TO 150-CP-102 FOR TRACKWORK, OVERPASS, UNDERPASS, TOLLBOOTH AND ROADWAY SECTION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
6. SEE DWGS 150-CP-100 TO 150-CP-142 FOR STORM DRAINAGE AND SURFACE WATER INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).
7. SEE DWGS 150-SEP-100 TO 150-SEP-119 AND 150-XY-001 TO 150-XY-003 FOR STRUCTURAL COLUMN AND WALL WITH FOUNDATION INFORMATION (SHOWN ON THIS PLAN FOR REFERENCE ONLY).




AB = ABANDON IN PLACE  
DESIGN-BUILD CONTRACTOR TO VERIFY IF ABANDONMENT IN  
PLACE IS ACCEPTABLE TO UTILITY OR CITY OF REDMOND)

ESP = SUPPORT/PROTECT DURING CONSTRUCTION  
MEANS AND METHODS TO BE DETERMINED BY THE DESIGN-BUILD  
CONTRACTOR AND UTILITY OWNER)

☒ **WORK TO BE PERFORMED BY OTHERS  
(DESIGN-BUILD CONTRACTOR TO COORDINATE)**

2. THE CONTRACTOR SHALL CONDUCT THEIR OWN RESEARCH AND INVESTIGATIONS WHICH IN THEIR PROFESSIONAL OPINION ARE NECESSARY TO EVALUATE THE ACCURACY OF THIS INFORMATION IN ADVANCE OF CONSTRUCTION WORK AND SUBSURFACE PENETRATIONS PERFORMED SO AS TO PREVENT THE IMPACT OF UTILITIES. THE INFORMATION PROVIDED HEREON MAY VARY FROM ACTUAL SIZES AND LOCATIONS.

AD = ADJUST TO NEW GRADE

CONCEPTUAL DESIGN	DESIGNED BY M. ALSTIN	DRAWN BY J. BETZOG	CHECKED BY D. PROCTOR	SUBMITTED BY V. VANG						LOCAL 1" = 20' 1" = 20' MAX REF: BUDGET/3110164 CONTRACT NO. C01146-1B DATE 03/26/2019	DOWNTOWN REDMOND LINK EXTENSION RTS TO DOWNTOWN REDMOND REFERENCE COMPOSITE UTILITIES PLAN EB STA 5.05+00 TO EB STA 5.187+72	DRAWING NO. <b>L90-UCP136</b> LOCATION ID SHEET NO. 278 SHEET TOTAL 278

DESIGNED BY	
M. AUSTIN	
DRAWN BY	
J. BETZVOG	
CHECKED BY	
D. PROCTOR	
APPROVED BY	
Y. YANG	

[illegible]











PRELIMINARY  
ENGINEERING

DRAWING NO.	L90-CDP133	LOCATION ID	SHEET NO.	REV.
			211	A

**DOWNTOWN REDMOND LINK EXTENSION**  
**RTS TO DOWNTOWN REDMOND**

REFERENCE

CIVIL  
 DRAINAGE PLAN  
 EB STA 5+150+00 TO EB STA 5+155+00

	SCALE
	1" = 20'
	P&E DRAWING
	FILE 159-COP-10070162
	CONTRACT ID NO
	CN ID 148-1B
	DATE
	03/26/2019

[illegible]

**Parametrix**  
 CONSULTING PLANNING & DESIGN SERVICES  
 10000 NE PHILLIPS AVE  
 SUITE 100  
 PORTLAND, OR 97220  
 503.253.0771  
 WWW.PARAMETRIX.COM

DECLASSIFIED BY T. NABOURS	
EXEMPT BY	
J. TYLER	
DECLASSIFIED BY	
J. HITCH	
APPROVED BY	
Y. YANG	

[illegible]

**CONSTRUCTION NOTES:**

1. CONNECT TO EXISTING STORM DRAIN STRUCTURE.
2. CONNECT TO EXISTING STORM DRAIN PIPE.
3. PROTECT EXISTING STORM DRAIN FACILITY.
4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE.
5. INSTALL AERIAL GUIDEWAY DRAIN.

**NOTES:**

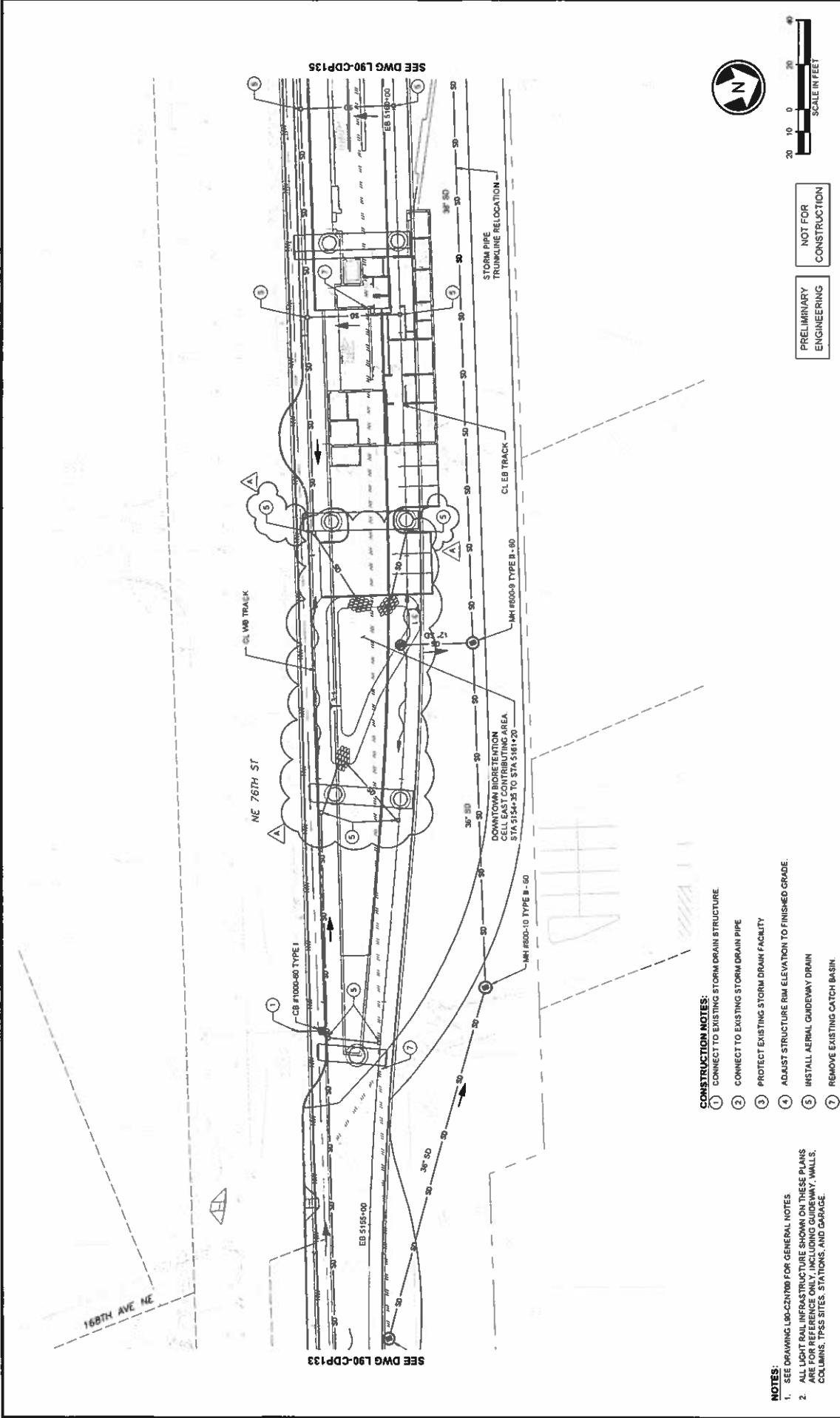
- 1 SEE DRAWING L90-C2N700 FOR GENERAL NOTES
- 2 ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

## CONCEPTUAL DESIGN

**CONFIRMED SET**

NO	DATE	DSN	CHK	APP	REVISION
A	02/01/19	NMS	BFP	BFP	ADDENDUM #6 - REVISION FOR TPSS #3





- CONSTRUCTION NOTES:**
1. CONNECT TO EXISTING STORM DRAIN STRUCTURE
  2. CONNECT TO EXISTING STORM DRAIN PIPE
  3. PROTECT EXISTING STORM DRAIN FACILITY
  4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE
  5. INSTALL AERIAL GUIDEWAY DRAIN
  6. REMOVE EXISTING CATCH BASIN

- NOTES:**
1. SEE DRAWING L90-CDP133 FOR GENERAL NOTES
  2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE



PRELIMINARY ENGINEERING  
NOT FOR CONSTRUCTION

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SCALE IN FEET

PRELIMINARY  
ENGINEERING

**NOTES:**

1. SEE DRAWING 190-22100 FOR GENERAL NOTES
2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS SHALL BE CONSTRUCTED TO THE FOLLOWING: TRACKS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.
3. REMOVE EXISTING CATCH BASIN
4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE
5. INSTALL AERIAL GUIDEWAY DRAIN
6. REMOVE EXISTING CATCH BASIN

**CONSTRUCTION NOTES:**

1. CONNECT TO EXISTING STORM DRAIN STRUCTURE
2. CONNECT TO EXISTING STORM DRAIN PIPE
3. PROTECT EXISTING STORM DRAIN FACILITY
4. ADJUST STRUCTURE RIM ELEVATION TO FINISHED GRADE
5. INSTALL AERIAL GUTTERWAY DRAIN
7. REMOVE EXISTING CATCH BASIN

**NOTES:**

2. ALL LIGHT RAIL INFRASTRUCTURE SHOWN ON THESE PLANS ARE FOR REFERENCE ONLY, INCLUDING GUIDEWAY, WALLS, COLUMNS, TPSS SITES, STATIONS, AND GARAGE.

CONCEPTUAL DESIGN	CONFORMED SET
-------------------	---------------

DESIGNED BY  
T. NABOURN  
DRAWN BY  
J. TYLER  
CHECKED BY  
J. HITCH  
APPROVED BY  
Y. YANG



**Parametrix**  
 11000 1st Avenue, Suite 100  
 San Diego, CA 92121  
 Tel: 619/594-1100  
 Fax: 619/594-1101  
 Email: info@parametrix.com



SCALE  
1" = 20'  
IN 1/4" SCALE  
RLE-L90-CDP100T0162  
CONTRACT NO.  
C/N 0148-18  
DATE  
03/29/2018

**DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND**

**REFERENCE**

DRAWING FILE  
**L90-CDP136**  
 COLLOCATION

Exhibit E

Cost Estimates

Betterment	DB Bid	ST Fixed Costs	RoW	Private Utility Relocations	City Contingency	Total Costs
51 ST PRV	\$ 430,000	\$ 61,052			\$ 97,500	\$ 588,552
70th St & 202	\$ 1,800,000	\$ 564,962		\$ 458,612	\$ 465,000	\$ 3,288,574
76th & 202	\$ 290,000	\$ 118,246			\$ 112,500	\$ 520,746
City RCC Portion	\$ 855,000	\$ 222,453	\$ 325,095		\$ 212,625	\$ 1,615,173
TOTAL	\$ 3,375,000	\$ 966,713	\$ 325,095	\$ 458,612	\$ 887,625	\$ 6,013,045

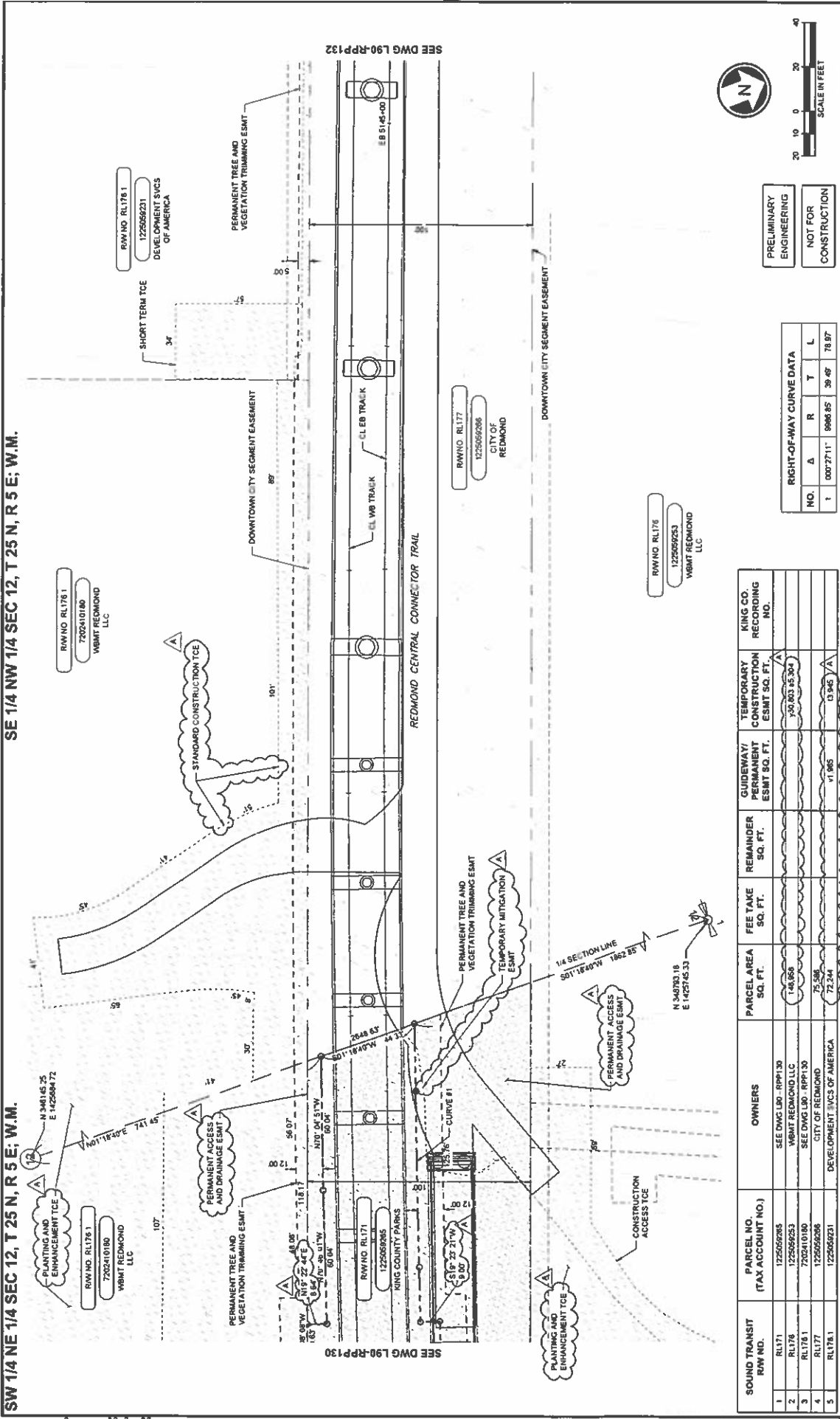
## **EXHIBIT F**

# NE 40th Street Undercrossing ROW



# Redmond Central Connector ROW





SOUND TRANSIT		PARCEL NO. (TAX ACCOUNT NO.)		OWNERS		FEE TAKE		REMAINDER		GUIDEWAY		TEMPORARY CONSTRUCTION		KING CO RECORDING	
NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE	NO.	DATE
1	03/15/19	RL171	122556221	SEE DWG L90-RPP130	SEE DWG L90-RPP130	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256
2	03/15/19	RL178	122556223	WBMAT REDMOND LLC	WBMAT REDMOND LLC	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256
3	03/15/19	RL178.1	122556221	SEE DWG L90-RPP130	SEE DWG L90-RPP130	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256
4	03/15/19	RL177	122556226	CITY OF REDMOND	CITY OF REDMOND	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256
5	03/15/19	RL176.1	122556221	DEVELOPMENT SVCS OF AMERICA	DEVELOPMENT SVCS OF AMERICA	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256	14,256

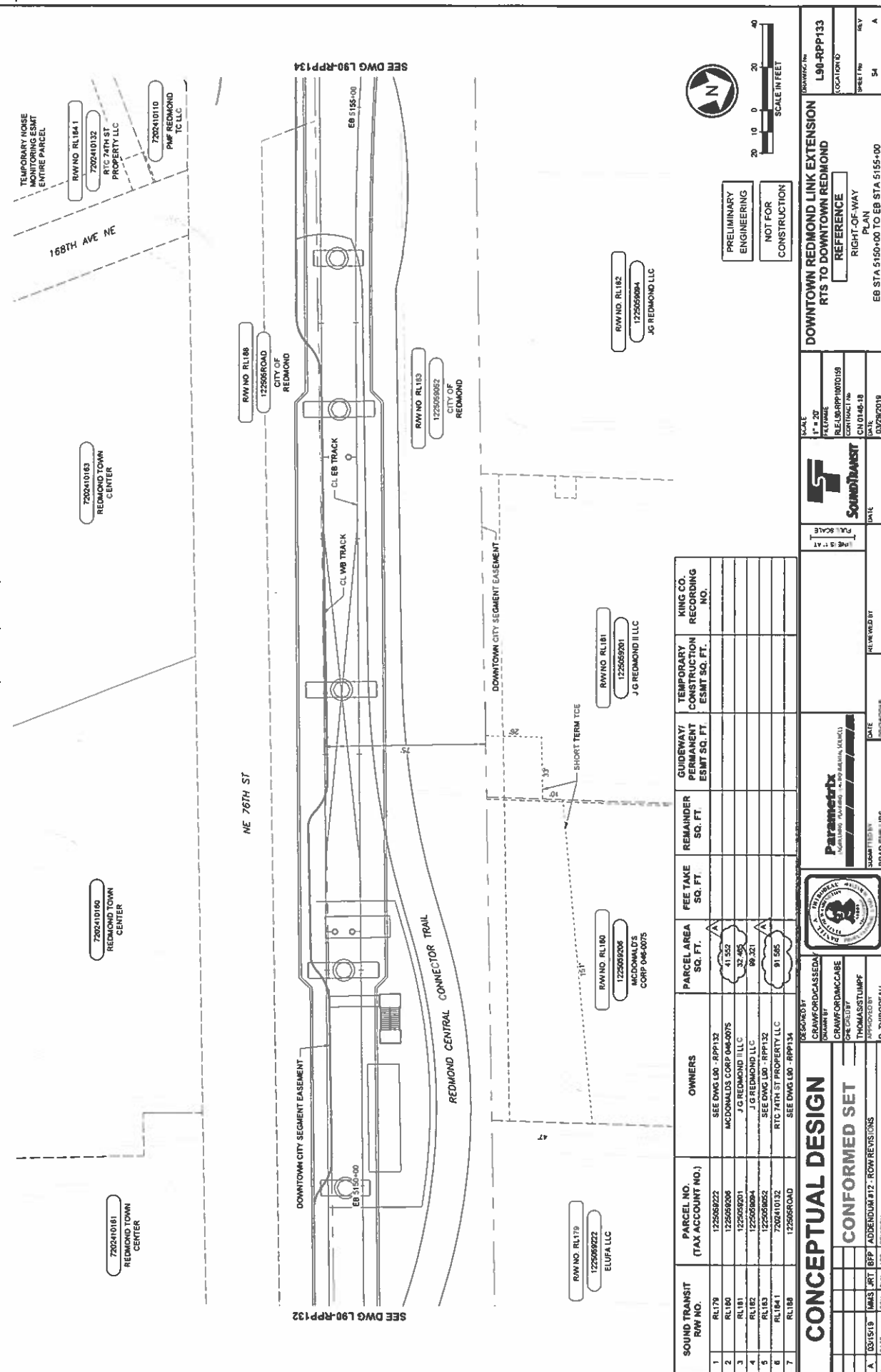
CONCEPTUAL DESIGN		CONFORMED SET	
NO.	DATE	NO.	DATE
1	03/15/19	1	03/15/19
2	03/15/19	2	03/15/19
3	03/15/19	3	03/15/19
4	03/15/19	4	03/15/19
5	03/15/19	5	03/15/19

DOWNTOWN REDMOND LINK EXTENSION		RTS TO DOWNTOWN REDMOND	
NO.	DATE	NO.	DATE
1	03/15/19	1	03/15/19
2	03/15/19	2	03/15/19
3	03/15/19	3	03/15/19
4	03/15/19	4	03/15/19
5	03/15/19	5	03/15/19

SOUND TRANSIT		PARAMETRIX	
NO.	DATE	NO.	DATE
1	03/15/19	1	03/15/19
2	03/15/19	2	03/15/19
3	03/15/19	3	03/15/19
4	03/15/19	4	03/15/19
5	03/15/19	5	03/15/19

DOWNTOWN REDMOND LINK EXTENSION		RTS TO DOWNTOWN REDMOND	
NO.	DATE	NO.	DATE
1	03/15/19	1	03/15/19
2	03/15/19	2	03/15/19
3	03/15/19	3	03/15/19
4	03/15/19	4	03/15/19
5	03/15/19	5	03/15/19







SOUND TRANSIT		PARCEL NO.	OWNERS	PARCEL AREA	FEE TAKE	REMAINDER	GUIDEWAY/ PERMANENT ESMT SQ. FT.	TEMPORARY CONSTRUCTION ESMT SQ. FT.	KING CO RECORDING NO.
R/W NO.		(TAX ACCOUNT NO.)		SQ. FT.	SQ. FT.	SQ. FT.			
1	RL182	1225550094	SEE DING L&O - RPP113						
2	RL133	1225550552	SEE DING L&O - RPP132						
3	RL144	1225550012	MOSE BRANK - MARLYN	25,908					
4	RL164	7702410113	SEE DING L&O - RPP133						
5	RL145	1225550170	GREY WINGATHER ENTERPRISES	24,523					
6	RL146	1225550282	SEE DING L&O - RPP125						
7	RL166	1225550540	CITY OF BENTON						

# CONCEPTUAL DESIGN

**CONFORMED SET**

A	03/15/19	WAVE	JRT	BFP	ADDENDUM #12 - ROW REVISIONS
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All of them are...

A 90% reduction in the number of cases was observed.

**CRAWFORD COMM-CABLE**

THOMAS/S TUMPF

## Parametrix

[illegible]

April 1999

[illegible]

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of All the Kings, So, the King

DATE	
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1

3-11-1997



1" = 20'  
DATE  
BY: NAME  
PROJECT: 4-90-RPP100T0159  
CITY/STATE/ZIP: CN 0148-15

**DOWNTOWN**  
**RTS TO**

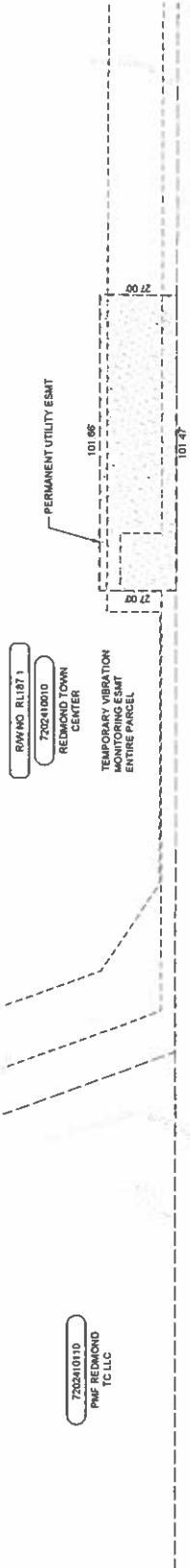
**REDMOND LINK  
DOWNTOWN REDEVELOPMENT  
REFERENCE  
RIGHT-OF-WAY  
PLAN**

**EXTENSION**  
**MOND**

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NW 1/4 NW 1/4 SEC 12, T 25 N, R 5 E; W.M.

SW 1/4 NW 1/4 SEC 12, T 25 N, R 5 E; W.M.



SEE DWG L90-RPP136

SEE DWG L90-RPP134



PRELIMINARY  
ENGINEERING  
NOT FOR  
CONSTRUCTION

RIGHT-OF-WAY CURVE DATA					
NO.	A	R	T	L	
1	100°00'00"	4.00'	4.00'	5.28'	

SOUND TRANSIT RAW NO.	PARCEL NO. (TAX ACCOUNT NO.)	OWNERS	PARCEL AREA SQ. FT.	FEE TAKE SQ. FT.	REMAINDER SQ. FT.	GUIDEWAY/ PERMANENT ESMT SQ. FT.	TEMPORARY CONSTRUCTION ESMT SQ. FT.	KING CO. RECORDING NO.
1	RL103	SEE DWG L90 - RPP132						
2	RL106	1225056002 FAIRWAY PROPERTY MANAGEMENT	32,692	1,362	31,000		12,636	
3	RL187	1225056007 CITY OF REDMOND						
4	RL187	7202410010 REDMOND TOWN CENTER	338,521				14,325	
5	RL188	1225056000 SEE DWG L90 - RPP134						

CONCEPTUAL DESIGN

CONFORMED SET

APPROVED BY: THOMAS STUMPF  
THOMAS STUMPF  
D. THIBODEAU



Parametrix  
ENGINEERS  
ARCHITECTS  
PLANNERS  
SCIENTISTS

SUBMITTED BY: BRAD PHILLIPS  
DATE: 08/24/2018

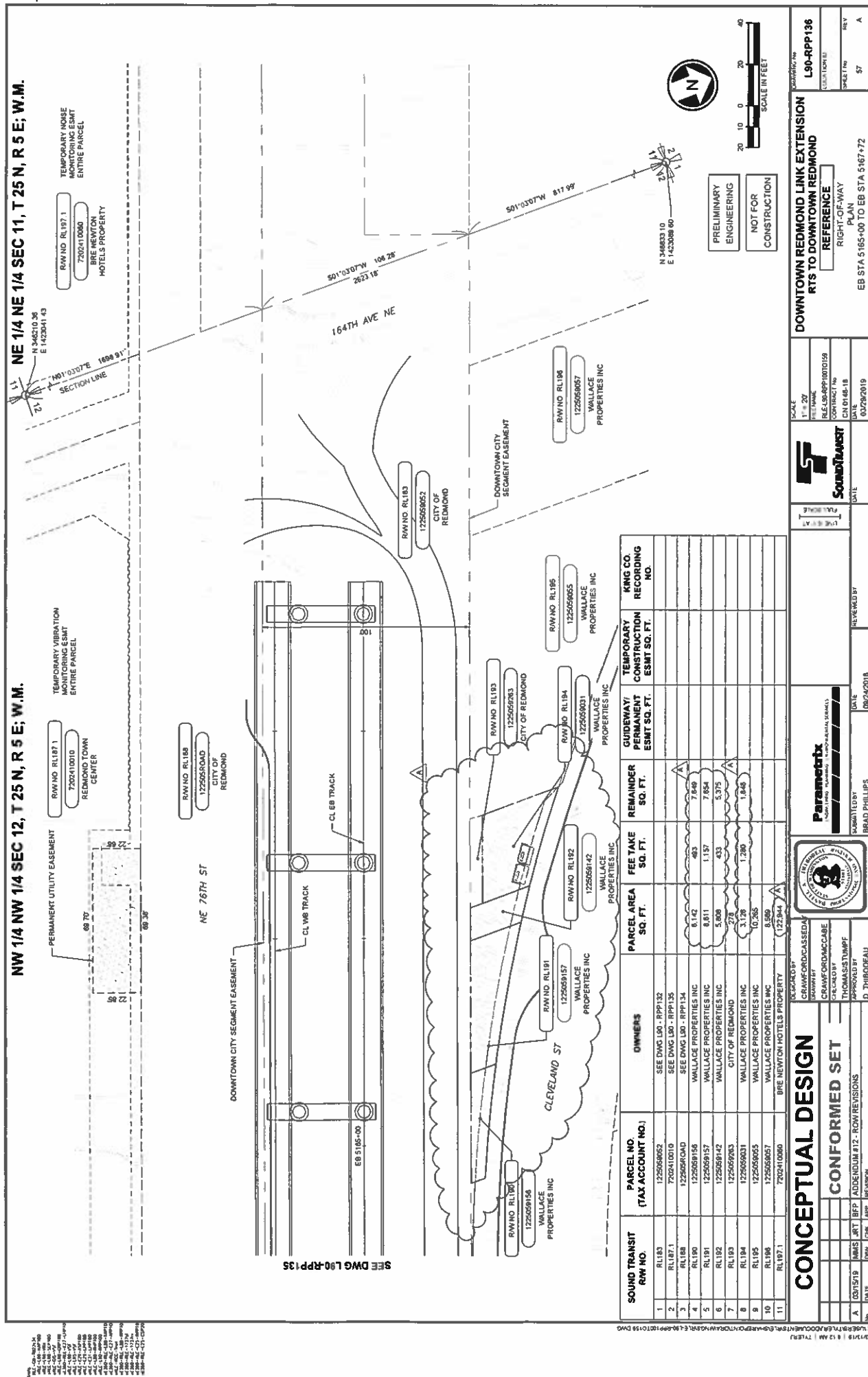
REVIEWED BY: [Signature]  
DATE: 08/24/2018



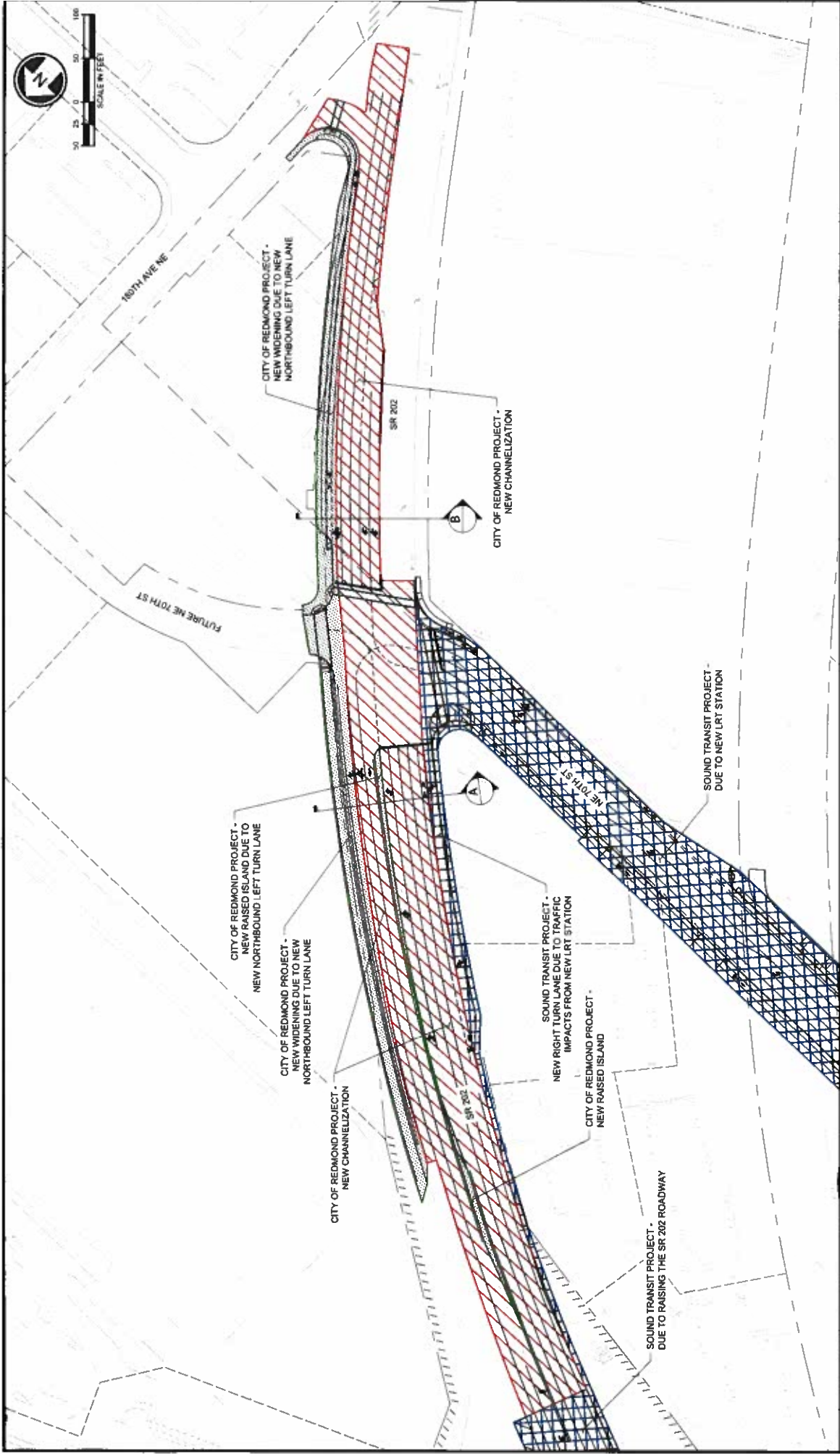
SCALE: 1" = 20'  
TITLE: DOWNTOWN REDMOND LINK EXTENSION  
PROJECT: DOWNTOWN REDMOND  
DRAWING NO.: L90-RPP135  
DATE: 03/26/2018

DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND  
REFERENCE  
RIGHT-OF-WAY  
PLAN  
EB STA 5160+00 TO EB STA 5165+00

LOCATION ID: L90-RPP135  
SHEET NO: 56  
REV: A



**NE 70th ST & SR 202 BETTERMENT**

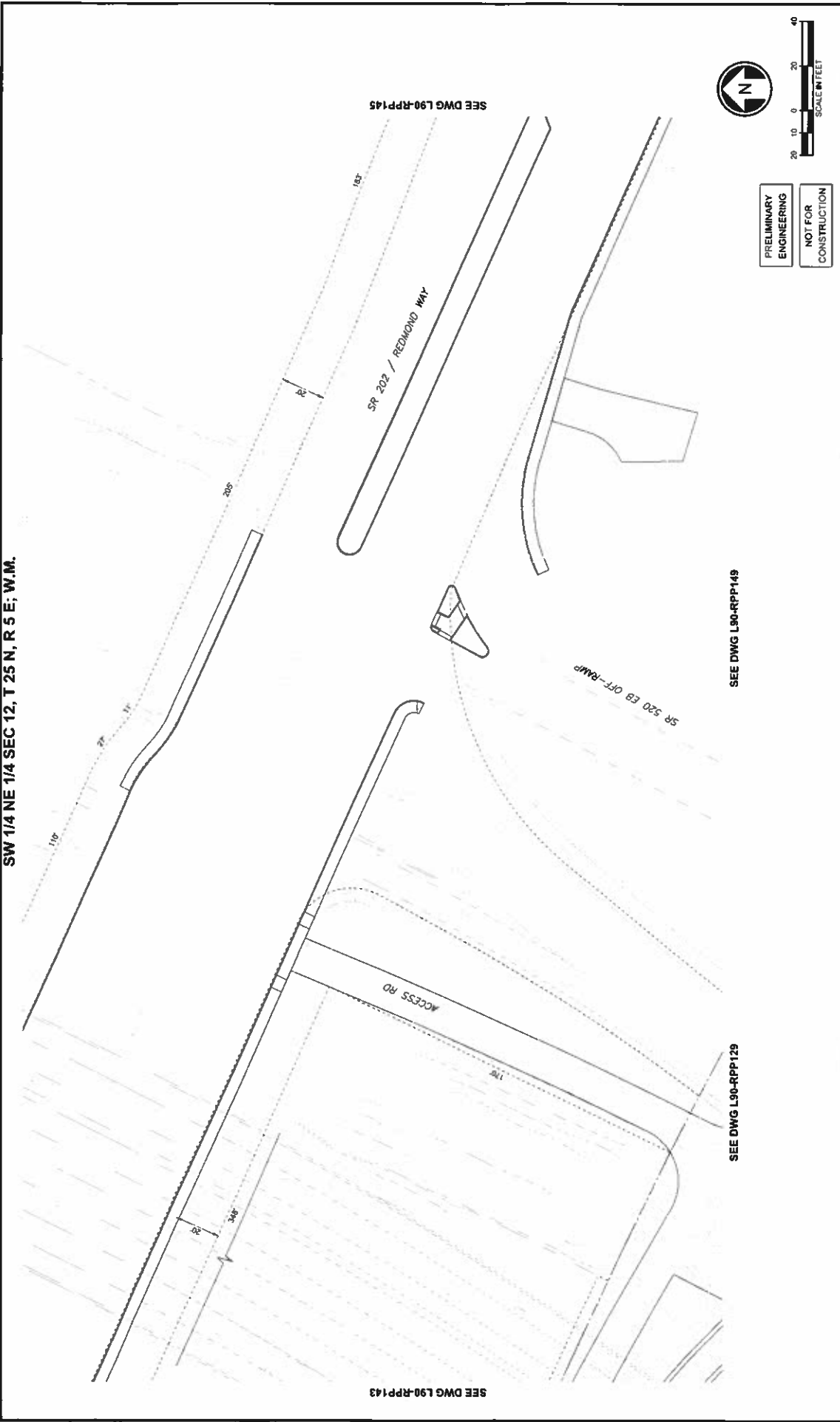


**LEGEND**

- SOUND TRANSIT WORK
- CITY OF REDMOND CIVIL WORK
- CITY OF REDMOND NEW CHANNELIZATION WORK



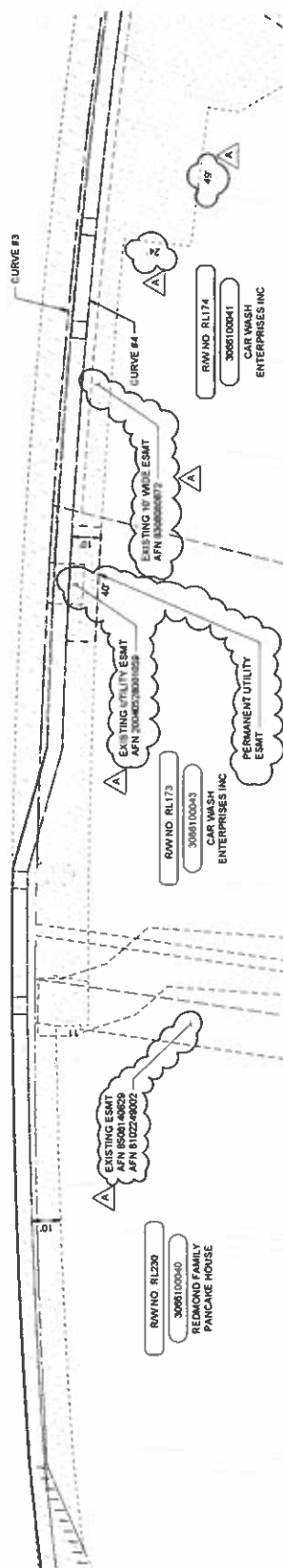
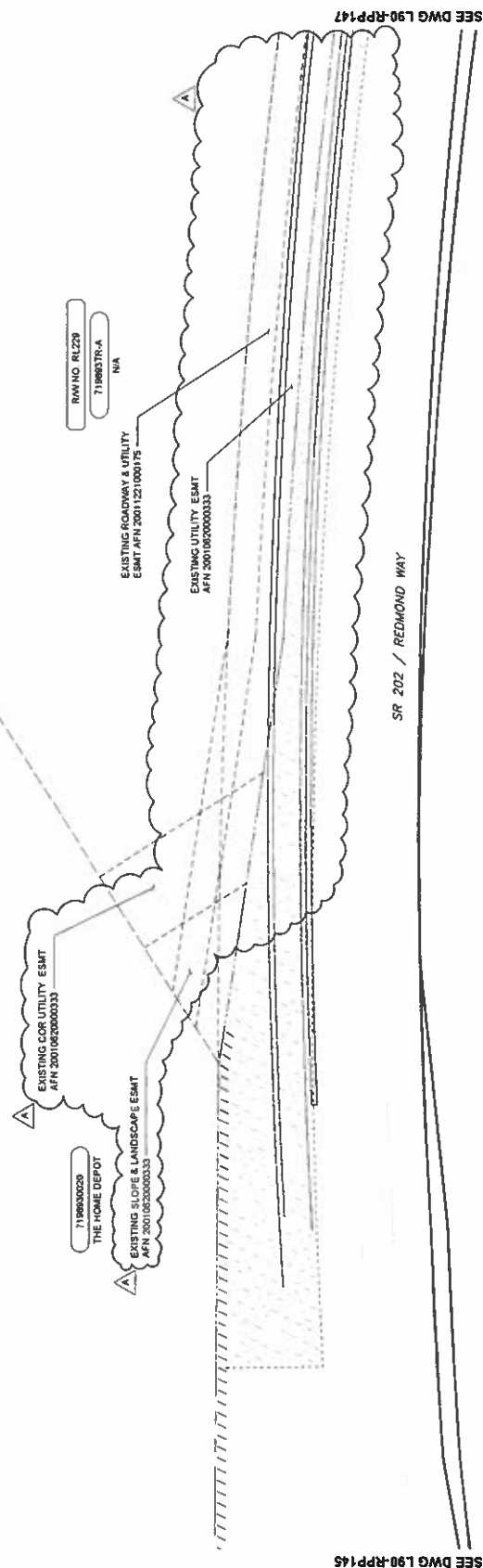
SW 1/4 NE 1/4 SEC 12, T 25 N, R 5 E, W.M.



PRELIMINARY  
ENGINEERING  
NOT FOR  
CONSTRUCTION

<b>CONCEPTUAL DESIGN</b>		<b>CONFORMED SET</b>		<b>REVISIONS</b>	
DATE	DESIGNED BY	CHECKED BY	APPROVED BY	DATE	DESCRIPTION
	CHANCE ORODCASSIDY	CRAWFORD MCCABE	THOMAS STIMPFF		
<b>Parametrix</b> Engineering & Construction, Inc. 10000 N. 19th Ave., Suite 100 Denver, CO 80202 Phone: (303) 751-1000 Fax: (303) 751-1001 www.parametrix.com					
<b>Sound Transit</b>		<b>DOWNTOWN REDMOND LINK EXTENSION</b> RTS TO DOWNTOWN REDMOND RIGHT-OF-WAY PLAN SR 202/REDMOND WY & SR 520 EB OFF-RAMP			
SCALE 1" = 20'		DATE 09/24/2018		SHEET NO. 65	
PROJECT NO. L90-RPP144		CONTRACT NO. CN 0146-18		SHEET NO. 65	
LOCATION NO.		DATE 03/28/2018		SHEET NO. 65	

ERADICATING THE ARAB BOYCOTT FROM THE JEWISH CANON



RIGHT-OF-WAY CURVE DATA				
NO.	A	R	T	L
3	007°33'23"	2815.00'	105.89'	371.25'
4	008°45'05"	2578.00'	151.95'	303.54'

SOUND TRANSIT RAW NO.		PARCEL NO. (TAX ACCOUNT NO.)	OWNERS	PARCEL AREA SQ. FT.	FEE TAKE SQ. FT.	REMAINDER SQ. FT.	GUIDEWAY/ PERMANENT ESMT SQ. FT.	TEMPORARY CONSTRUCTION ESMT SQ. FT.	KING CO. RECORDING NO.
1	RL173	3066100043	CAR WASH ENTERPRISES INC	37,297	2,010	35,287	0	0	
2	RL174	3066100041	SEE DWG 100_RPFI42						
3	RL229	71068378-A		13,941					
4	RL230	71068378-A							

CONCEPTUAL DESIGN		RECORDED BY CRAWFORD/CASSEDAY		DATE 11/1/20		PROJECT BELLINGHAM RD 001159		DRAWING NO. L90-RPP-146	
CONFORMED SET		DRAWN BY CRAWFORD/CASSEDAY		SCALE 1" = 10'		SHEET NO. 67		SHEET TOTAL 67	
A 03/15/19		JMS JRT		BFP		ADDITIONAL #12, ROW REVISIONS		PLAN SR 00229 REDMOND WY	

## DOWNTOWN REDMOND LINK EXTENSION

SCALE  
0 100  
cm

[illegible]

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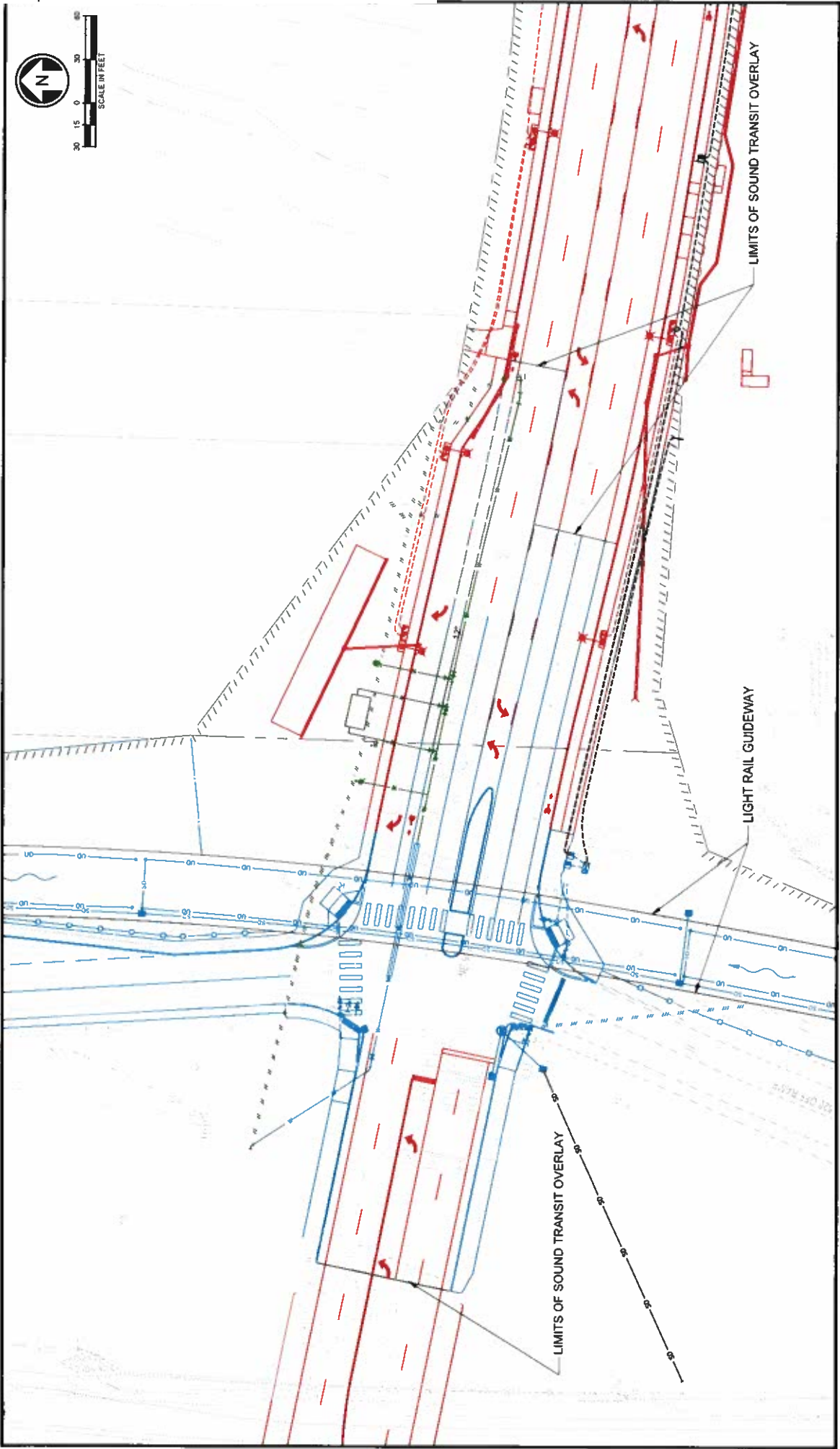
## CONCEPTUAL DESIGN







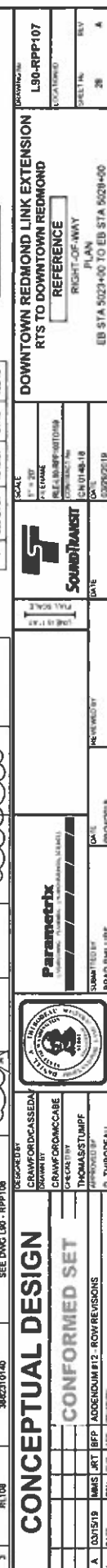
**NE 51st ST WATER MAIN BETTERMENT**



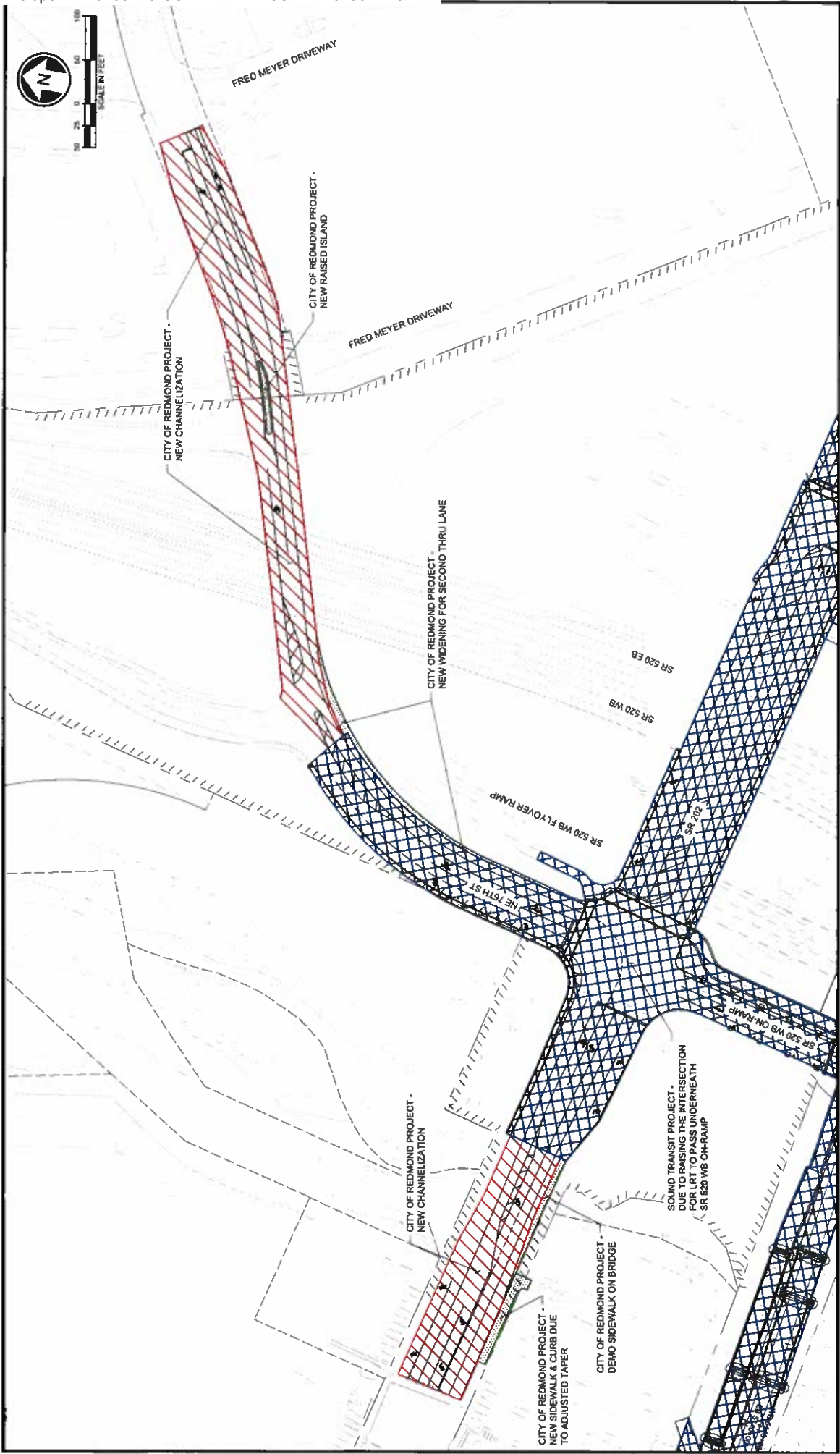
**LEGEND**

- WORK DONE BY CITY OF REDMOND
- WORK DONE BY SOUND TRANSIT
- WORK DONE BY SOUND TRANSIT, PAID FOR BY CITY OF REDMOND





SR 202 & NE 76th ST BETTERMENT

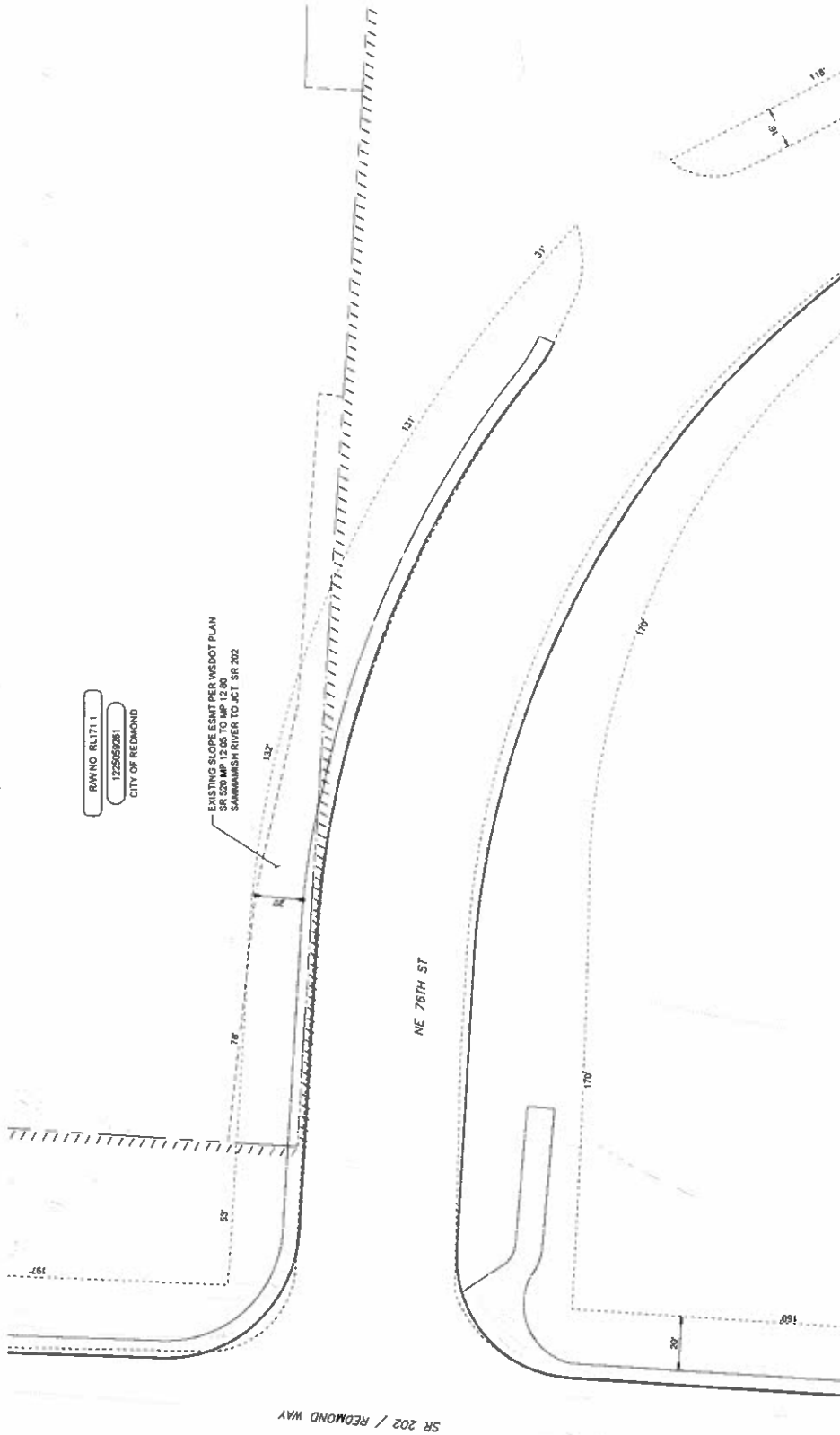


**ATTACHMENT 1**  
**SR 202 & NE 76TH STREET ROADWAY IMPROVEMENTS**  
**DOWNTOWN REDMOND LINK EXTENSION**



- LEGEND**
- SOUND TRANSIT WORK
  - CITY OF REDMOND NEW CHANNELIZATION WORK (BETTERMENT)
  - CITY OF REDMOND CIVIL WORK (BETTERMENT)

SW 1/4 NE 1/4 SEC 12, T 25 N, R 5 E; W.M.



RAWNO RL1711  
1226559281  
CITY OF REDMOND

EXISTING SLOPE ESMT PER WSDOT PLAN  
SR 202 M 12.85 TO M 12.8  
SANDWICH RIVER TO SET SR 202

NE 76TH ST

SR 202 / REDMOND WAY

SEE DWG L90-RPP143

SEE DWG L90-RPP163



PRELIMINARY  
ENGINEERING  
NOT FOR  
CONSTRUCTION



SOUND TRANSIT ROW NO.	PARCEL NO. (TAX ACCOUNT NO.)	OWNERS	PARCEL AREA SQ. FT.	FEE TAKE SQ. FT.	REMAINDER SQ. FT.	GUIDEWAY/ PERMANENT ESMT SQ. FT.	TEMPORARY CONSTRUCTION ESMT SQ. FT.	KING CO. RECORDING NO.
1	RL1711	CITY OF REDMOND	249,014	249,014			13,750	

## CONCEPTUAL DESIGN

CONFORMED SET

DATE	BY	APP	REV
03/15/19	MAN: JRT	BFP	ADDENDUM #12: ROW REVISIONS



Parameterix  
PARAMETERIX ENGINEERING, INC.

DATE: 09/24/2018  
BRAD PHELIPS



SCALE  
1" = 20'  
DATE: 03/28/2019  
DATE: 03/28/2019

DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND

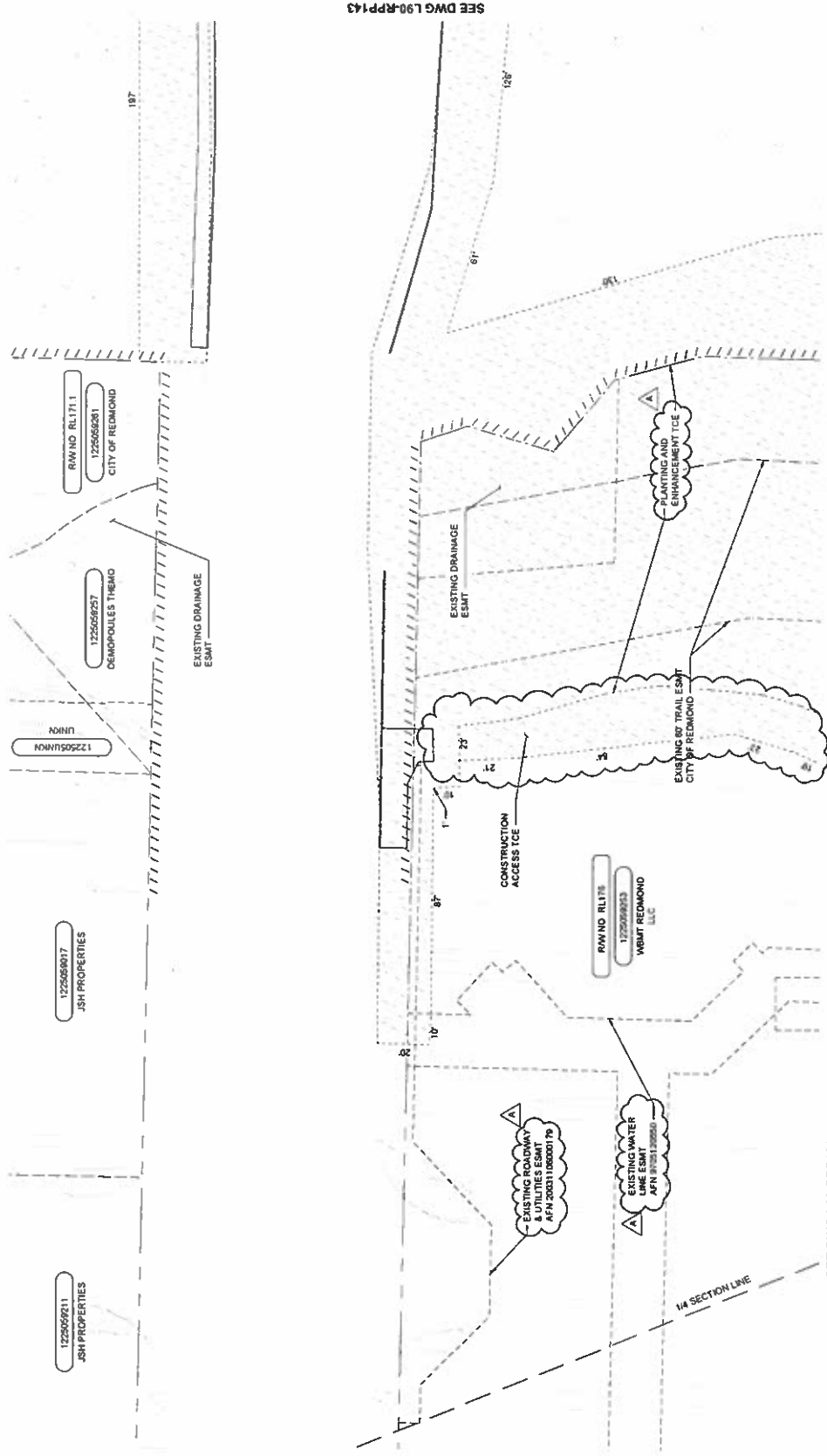
REFERENCE  
RIGHT-OF-WAY  
PLAN

NE 76TH ST & SR 202/REDMOND WY INTERSECTION

PROJECT NO.  
L90-RPP162  
SHEET NO.  
83  
REV  
A



SW 1/4 NE 1/4 SEC 12, T 25 N, R 5 E; W.M.



PRELIMINARY  
ENGINEERING  
NOT FOR  
CONSTRUCTION

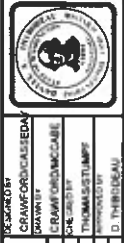


SOUND TRANSIT RW NO.	PARCEL NO. (TAX ACCOUNT NO.)	OWNERS	PARCEL AREA SQ. FT.	FEE TAKE SQ. FT.	REMAINDER SQ. FT.	GUIDEWAY/ PERMANENT ESMT SQ. FT.	TEMPORARY CONSTRUCTION ESMT SQ. FT.	KING CO RECORDING NO.
1. RL171	122505001	SEE DWG L90-RPP162						
2. RL176	122505003	SEE DWG L90-RPP131						

CONCEPTUAL DESIGN

CONFORMED SET

DATE	DESIGN	APP	REVISION
03/15/19	MAS	JT	1



Parametrix  
INCORPORATED  
10000 152nd Ave SE  
Burien, WA 98148  
P: 206.835.1234  
F: 206.835.1235  
www.parametrix.com

DATE: 09/26/2018



SCALE: 1" = 20'  
DATE: 09/26/2018  
BY: J. THIBODEAU  
CHECKED: J. THIBODEAU  
DATE: 09/26/2018

DOWNTOWN REDMOND LINK EXTENSION  
RTS TO DOWNTOWN REDMOND  
REFERENCE  
RIGHT-OF-WAY  
PLAN  
SR 202 REDMOND WY & BEAR CREEK

Drawn By: L90-RPP142  
Checked: J. THIBODEAU  
Scale: 1" = 20'  
Date: 09/26/2018

**EXHIBIT G**

**DESIGNATED REPRESENTATIVES**

**SOUND TRANSIT:**

Brian Holloway  
East Corridor Design Manager  
401 South Jackson Street  
Seattle, Washington 98104  
Phone: 206-903-7421  
Email: brian.holloway@soundtransit.org

**CITY OF REDMOND:**

Patty Criddle  
Senior Engineer  
PO Box 97010  
Redmond, WA 98073-9710  
Telephone: 425-556-2736  
Email: pscriddle@redmond.gov



**Connected Community  
Enhanced Livability  
Environmental Sustainability**

June 25, 2024

Tony Raben  
Project Director  
Sound Transit  
201 South Jackson ST.  
Seattle, WA 98105

Re: Redmond Central Connector (RCC) See Loc 11  
**Change to Project Scope Request -Rev1**

Dear Mr. Raben:

To facilitate development adjacent to the RCC Betterment project the City is requesting the following revisions to the Project scope:

Changes to the Project scope are shown the attached plan sheets markups.

- Red areas
  - o DELETED SCOPE:
    - Remove all work related to surface improvements including irrigation installation, plantings, soil amendments, and concrete modules from the project scope.
    - Sound Transit will not be held responsible for damage to facilities already installed (such as the storm system) by construction activities performed after the transfer of responsibility.
    - Pedestrian lighting is not included in this change to the project Scope.
  - o ADDED SCOPE:
    - Install a minimum of 3" of Crushed Surfacing Top Course. The City will consider this area fully stabilized for purposes related to City Municipal NPDES Permit.
- Blue area
  - o ADDED SCOPE
    - Install a temporary irrigation main, valves to the planter areas and connect to the City irrigation system.
    - Sound Transit will not be responsible for the maintenance of this temporary irrigation main. Any plantings damaged due to failure to maintain the temporary water main will be removed from the one-year warranty.

**City Hall**

15670 NE 85th Street  
PO Box 97010  
Redmond, WA  
98073-9710



June 25, 2024

Page 2

- Orange and Grey Areas
  - o DELETED SCOPE:
    - Do not install pavers.
    - Exact limits of the scope revision to be identified by the City.
    - This area will not be included in the one-year warranty.
  - o ADDED SCOPE:
    - Install (2) 2'X10" Concrete Bands across the RCC.
    - Install 6-inch thick concrete base slab
    - Install 3.65 inches of HMA PG CL58H-22 over concrete with a bond breaker.
- Plantings removed from the Red Area will be considered as installed for the purposes related to the Tree Mitigation Plan.
- The transfer of responsibility for the area will take place once the RCC Project is Physically Complete but not before 2025. The transfer can happen earlier if Sound Transit determines work is complete in the area.
- 3<sup>rd</sup> Party facilities, such as the PSE duct bank, are known and will be identified and protected prior to future work in the area taking place.
- The areas will be restored by others in a manner consistent with City Standards.
- The City does not request or expect that the requested scope changes will result in any financial credit or expense for described scope revisions to the RCC project.

If you have any questions, please contact Aaron Noble, the City's Project Manager at (425) 556-2792, [abnoble@redmond.gov](mailto:abnoble@redmond.gov).

Sincerely,

Vangie Garcia, PE, PMP  
Deputy Director - Infrastructure and Mobility

*Vangie Garcia*

6/25/2024

Attachments:

Attachment A- Plan Sheet Markups

**FIRST AMENDMENT**  
**TO**  
**AGREEMENT**  
**BETWEEN CITY OF REDMOND AND THE CENTRAL PUGET SOUND**  
**REGIONAL TRANSIT AUTHORITY**  
**FOR THE FUNDING, DESIGN, AND CONSTRUCTION OF**  
**DOWNTOWN REDMOND LINK EXTENSION BETTERMENTS**

This First Amendment to Agreement Between City of Redmond and the Central Puget Sound Regional Transit Authority for the Funding, Design, and Construction of Downtown Redmond Link Extension Betterments (the “First Amendment”) is made effective as of the date of latest signature, below, by and between the Central Puget Sound Regional Transit Authority, a regional transit authority of the State of Washington (“Sound Transit”) and the City of Redmond, a Washington municipal corporation (the “City”) (each, individually, a “Party” and, together, the “Parties”).

**RECITALS**

- A. Pursuant to that certain “Agreement Between City of Redmond and the Central Puget Sound Regional Transit Authority for the Funding, Design, and Construction of Downtown Redmond Link Extension Betterments” dated October 18, 2019 (GA 0143-19) (the “Betterment Agreement”), Sound Transit agreed to design and construct improvements to the City’s Redmond Central Connector Trail (the “RCC Improvements”), subject to certain cost sharing between the Parties.
- B. Exhibit D to the Betterment Agreement included the conceptual design for the RCC Improvements, including specified plantings and finish levels.
- C. Pursuant to that certain “Light Rail Easement Agreement (Redmond Spur Rail Corridor—Downtown City Segment)” dated April 11, 2012 (the “Sound Transit Easement”), Sound Transit acquired the right to construct, operate, and maintain its light rail transit facilities for the Downtown Redmond Link Extension Project on certain City property, including property upon which Sound Transit is to construct the RCC Improvements under the Betterment Agreement.
- D. The owner and developer of property located adjacent to the portion of the Sound Transit Easement between 170<sup>th</sup> Avenue Northeast and Downtown Redmond Station and upon which a portion of the RCC Improvements are to be constructed. The developer desires to use a portion of the Sound Transit Easement area adjacent to its property for construction staging (the “Construction Staging Area”), and the City desires to allow same.

- E. The City has requested that a portion of the RCC Improvements be finished to a level appropriate for the developer's use for construction staging instead of being finished to the level identified in the Betterment Agreement, and Sound Transit agrees to same.
- F. Accordingly, the Parties desire to amend the Betterment Agreement to reflect the above-described scope change.

### AGREEMENT

NOW, THEREFORE, the Parties agree as follows:

1. Exhibit D: Redmond Central Connector Scope and Conceptual Design, is hereby supplemented with the attached Exhibit D.1.
2. All other provisions of the Betterment Agreement share remain in full force and effect.

IN WITNESS WHEREOF, the Parties have executed this First Amendment as of the date of latest signature, below.

CENTRAL PUGET SOUND  
REGIONAL TRANSIT AUTHORITY

CITY

By \_\_\_\_\_

By \_\_\_\_\_

Title \_\_\_\_\_

Title \_\_\_\_\_

Date: \_\_\_\_\_

Date: \_\_\_\_\_

Approved as to form:

Approved as to form:

By: \_\_\_\_\_  
Natalie Moore, Senior Legal Counsel

By: \_\_\_\_\_

**Attachments:**

Exhibit D.1: Scope Revision at RCC Betterment



February 3, 2025

Scott A. Koppelman  
PPF AMLI Redmond Way, LLC  
141 W Jackson Blvd, STE 300  
Chicago, IL, 60604-2992

***Subject: Letter of Agreement RE: 16771 Redmond Way, Redmond WA - Access and Surface Improvements***

Dear Scott Koppelman:

By this Letter of Agreement (“Agreement”), Sound Transit approves PPF AMLI Redmond Way, LLC, a Delaware limited liability company (“Licensee”)’s application to access and install surface improvements on property occupied by the Sound Transit Link Light Rail System (“Link System”) in the public right of way in the vicinity of 16771 Redmond Way, WA 98052 (the “License Area,” as depicted on **Exhibit A**) for the purposes described and pursuant to the conditions set forth in this Agreement.

The City of Redmond (the “City”) owns the License Area and Sound Transit has the non-exclusive right to construct, operate, maintain, and own its Link light rail transit system for the Downtown Redmond Link Extension project (the “Project”) thereon pursuant to that certain “Transit Way Agreement for the Downtown Redmond Link Extension Project City of Redmond, Washington,” authorized by the Redmond City Council on July 16, 2019, as amended (the “Transit Way Agreement”) and that certain “Light Rail Easement Agreement (Redmond Spur Rail Corridor—Downtown City Segment)” recorded in King County under Recording Number 20120411001176, as amended (the “Easement”). The City has issued permit no. SITE-2023-02147, dated December 7, 2023, to Licensee (the “City Permit”) for the purpose of installing Licensee’s Facilities, as defined below in this Agreement.

Pursuant to the April 2019 “Intergovernmental Agreement between Sound Transit and King County for the Operations and Maintenance of the Link Light Rail System,” King County, through its Metro Transit Department (the “County”), operates and maintains the Link System on Sound Transit’s behalf.

Consistent with the foregoing, Sound Transit and Licensee agree as follows:

1. Use of License Area. Licensee may use the License Area as a construction access “haul route” and to install fire safety, landscaping, pedestrian pathway, and other surface improvements (the “Facilities”) as further depicted and described on **Exhibit B** (the “Permitted Activities”) for as long as this Agreement remains in effect. Licensee agrees that its use of the License Area shall not interfere with the normal operations of the License Area, Sound Transit, or any portion of the Link System. Without limiting the foregoing, Licensee shall, at no time during its use of the License Area, interfere with general purpose vehicular ingress and egress to and from the License Area. Licensee agrees that its use of the License Area shall not interfere with electrically controlled light rail signals, telephone, or other circuits of the Link System, or with any telephone or other company or person operating circuits on the License Area or along the tracks. All of Licensee’s activities within the License Area under this Agreement shall comply with any standards imposed on those activities by the City.
2. Term; Termination. The rights and obligations conferred by this Agreement shall commence on the effective date of the City Permit and shall terminate immediately upon the earlier of: (1) termination of the City Permit; or (2) Licensee’s removal of the Facilities from the License Area. Notwithstanding the foregoing, Sound Transit may terminate this Agreement upon 30 days’ advance written notice to

Licensee.

3. Conditions of Use.

- a. Licensee (including Licensee's duly authorized representatives, employees, and agents) will not permit any other party, except Licensee's independent contractors performing the Permitted Activities on the License Area ("Contractors") to enter or use the License Area. Licensee will be responsible for its Contractors' compliance with the terms of this Agreement and shall provide such Contractors with written notice of the requirements contained in this Agreement prior to their entry onto the License Area. Licensee shall be responsible for its Contractors' compliance with all terms and conditions of this Agreement.
- b. Licensee shall coordinate the proposed work schedule with Sound Transit Property Management at [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) and, if the License Area is being used for Link operations at the time of the request, Sound Transit's Link Control Center ("LCC") at [LinkTrackAccess@soundtransit.org](mailto:LinkTrackAccess@soundtransit.org) or 206.205.8177 to avoid any conflicts with Sound Transit's construction and/or operations.
- c. Licensee shall not, and shall prohibit its Contractors from, parking any vehicles, placing any equipment, or storing any materials on the License Area within 10 feet of the track.
- d. Licensee shall not, and shall prohibit its Contractors from, parking any vehicles, placing any equipment, or storing any materials on the License Area without first obtaining Sound Transit's prior written approval, which approval Licensee shall obtain by contacting Sound Transit Property Management at [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) and, if the License Area is being used for Link operations at the time of the request, LCC at [LinkTrackAccess@soundtransit.org](mailto:LinkTrackAccess@soundtransit.org) or 206.205.8177.
- e. Licensee shall contact the applicable one-number locator service and Sound Transit Facilities at 206.553.3789 or [LinkFacilitiesST@soundtransit.org](mailto:LinkFacilitiesST@soundtransit.org) to locate existing underground utilities, transmission lines, and private fiber optics communications lines. Licensee shall be responsible for any damage to any utility caused by Licensee or its Contractors in violation of Chapter 19.122 RCW.
- f. Licensee shall be responsible for the proper removal and disposal of any debris and trash resulting from its or its Contractors' use of the License Area. Materials such as mud, soils, cutting slurry, etc. shall be collected and disposed of at an appropriate disposal site. Neither Licensee nor its Contractors shall dispose of hazardous materials on the ground surface of the License Area. Licensee shall be responsible for cleaning and remediating damage to the Licensee arising from any source arising out of its use of the License Area.
- g. Licensee will not engage in, and shall prohibit its Contractors from engaging in, any loud or objectionable behavior in the License Area.
- h. All activities on the License Area pursuant to this Agreement shall comply with all federal, state, and local laws and all Sound Transit rules and regulations. Illegal activities are prohibited under this License.
- i. Licensee shall be responsible for any damage done to the License Area by Licensee or its Contractors. Prior to commencing and while performing the Permitted Activities, Licensee shall identify and protect all existing improvements within the License Area.
- j. Neither Licensee nor its Contractors will interfere with Sound Transit, the County, or those entities' authorized representatives' access to the License Area.

- k. Licensee accepts the License Area in an “as-is” condition. Sound Transit has no obligation to provide HVAC, water, sewer, or other utilities. Sound Transit has no obligation to provide restroom facilities or restroom supplies.
  - l. Licensee and its Contractors are not permitted to block any fire safety system component, emergency exits, security cameras, maintenance access points, ticket vending machines, information boards or emergency call boxes. Licensee must conduct the Permitted Activities a minimum of 15 feet from any stairway, escalator, elevator, entrance, or customer service counter.
  - m. Licensee will keep the License Area secured at all times when not in use to prevent access by anyone other than Licensee.
4. Contractor Approval / Track Access. Prior to Licensee’s exercise of any rights under this License, Licensee must, and shall require its Contractors to:
- a. If the License Area is in the care, custody and control of Sound Transit’s contractor: Obtain written approval from Stacy and Witbeck/Kuney by contacting Gavino Rodriguez at [grodriguez@stacywitbeck.com](mailto:grodriguez@stacywitbeck.com), or such other contact as Sound Transit may hereinafter designate (the “Sound Transit Contractor”). Licensee shall coordinate with the Sound Transit Contractor to schedule Licensee’s access to the License Area and shall comply with any reasonable conditions imposed by the Sound Transit Contractor on Licensee’s performance of the Permitted Activities in the License Area.
  - b. If the License Area is being used for Link light rail operations: Apply for a Link track access permit by contacting [LinkTrackAccess@soundtransit.org](mailto:LinkTrackAccess@soundtransit.org).
5. Construction of Facilities. The Facilities shall be constructed by a licensed contractor in accordance with drawings approved in advance by Sound Transit and in accordance with all laws, rules, regulations, ordinances, and requirements of governmental agencies, offices, and boards having jurisdiction.
6. As-Built. An engineered plan including the project location, applicable notes, survey control, and the full extent of work, including sections and applicable details shall be submitted to [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) prior to engaging in any of the Permitted Activities on the License Area. Plans not readable as determined by Sound Transit shall be rejected.

Within 90 days of work completion, “as-built” documents based on the plans earlier provided, shall be submitted to [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) that show the Facilities as constructed.

The as-built documents shall:

- a. Include, at a minimum:
  - i. Project Cover Sheet.
  - ii. Plans, profiles, and details.
  - iii. Abbreviations and symbols sheet.
  - iv. Horizontal and vertical control notes and plans. (Note: For substantial projects occurring primarily within Sound Transit Right-of-Way, Licensee shall coordinate with a Sound Transit utility engineer regarding use of Sound Transit datum standards.)
  - v. Northing and Easting calls on each drawing sheet. Indicate Northing and Easting calls for all angle points, 3 points on any curve and a minimum of two calls on a sheet with a straight line.

- vi. Combined conversion factors to convert as-built data to Washington State Plan Coordinate system (US. Foot Coordinates).
- vii. Identification of size, material, quantity, and type (water, sewer, storm power, fiber optics) of facility.
- b. Be submitted as full-size, single sheet PDFs, at 300dpi min. and in the current version AutoCAD format and include all files needed to reproduce the drawings from CAD.
- 7. Permits and Licenses. Licensee is solely responsible for securing, and for requiring its Contractors to secure, any and all required permits, franchises, and licenses (“Permits”) at Licensee’s expense. Consistent with Licensee’s indemnity obligations in this License, Licensee shall be solely responsible for and any costs, damages, or liabilities arising from Licensee or its Contractors’ failure to obtain any Permits.
- 8. Entry by Licensee.
  - a. At least three days prior to commencing any Permitted Activities on the Property, Licensee shall notify Sound Transit’s Property Management Department at [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) to coordinate its work schedule with Sound Transit and to submit a copy of any required Link Track Access Permit(s).
  - b. After the initial entry during any period of construction, Licensee shall notify Property Management at [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) each morning prior to its or its Contractors’ entry onto the Property, or at other such interval as may be mutually agreed upon by the Parties.
  - c. After initial construction of the Facilities, when Licensee or its Contractors perform non-emergency work on the Facilities, Licensee shall notify Sound Transit Property Management at [propertymanagement@soundtransit.org](mailto:propertymanagement@soundtransit.org) at least 48 hours in advance of the desired access and shall coordinate its work schedule with Sound Transit. If Licensee must make emergency repairs to its Facilities, Licensee shall contact LCC at 206-205-8177 to obtain immediate access and to initiate coordination with Sound Transit in response to the emergency. Licensee may make emergency repairs to its Facilities as described herein only in instances when there is immediate danger to human health and safety.
  - d. Sound Transit may implement other reasonable conditions for Licensee’s and its Contractors’ access to the License Area.
- 9. Safety. Licensee shall conduct, and require its Contractors to conduct, their activities in the License Area in a safe manner. Licensee shall be solely responsible for the safety of all persons and property during its and its Contractors’ use of the License Area. Licensee shall maintain the Facilities in conformity with all applicable laws and regulations.
- 10. Restoration and Removal. Licensee shall, after any installation, construction, maintenance, repair, relocation, Permitted Activities, or removal of the Facilities, promptly restore the License Area to as good or better condition than that which existed prior to the commencement of such work and consistent with any additional City or Sound Transit standards hereinafter identified by those entities. Upon termination of this License, or if Licensee abandons the Facilities, Licensee shall remove the Facilities at its own expense. If Licensee fails to remove the Facilities consistent with its obligations contained herein, Sound Transit may remove the Facilities and charge Licensee for the cost of same.
- 11. Reservation of Rights; Relocation and Removal of Facilities. Sound Transit reserves unto itself the right to license, lease, or otherwise grant rights within the License Area to others and this Agreement shall not prohibit or prevent Sound Transit from using the License Area. Licensee agrees that Sound Transit may, for any reason, including, but not limited to, its own proposed use of the License Area, require Licensee to relocate or remove the Facilities upon 30 days’ advance written notice. Such relocation or removal shall be

conducted at Licensee's sole expense and shall be subject to the restoration provisions of the foregoing section.

12. Indemnification. Licensee agrees to defend, indemnify, and hold harmless Sound Transit and its officers, directors, agents, and employees and the County and its officers, directors, agents, and employees (the "Indemnified Parties") from and against any and all claims, demands, or causes of action and the resulting losses, costs, expenses, reasonable attorney's fees, liabilities, damages, orders, judgments, or decrees ("Claims") arising out of the acts, errors, or omissions of Licensee or its Contractors related to or in any way arising out of the Permitted Activities; the installation, operation, maintenance, repair, or removal of the Facilities; Licensee's or its Contractors' failure to obtain necessary property rights and/or permission to install, operate, or maintain the Facilities or to use the License Area; or Licensee's breach of this License. To the extent that RCW 4.24.115 applies, the indemnity provisions provided herein shall be valid and enforceable only to the extent of the negligence of Licensee and/or its Contractors and shall not apply in the event that any Claims arise out of Sound Transit's sole negligence. Licensee specifically assumes potential liability for any claim, demand, and/or cause of action brought by, or on behalf of, any of its employees or agents against the Indemnified Parties. FOR THIS PURPOSE, LICENSEE, BY MUTUAL NEGOTIATION, HEREBY WAIVES, WITH RESPECT TO THE INDEMNIFIED PARTIES ONLY, ANY IMMUNITY THAT WOULD OTHERWISE BE AVAILABLE TO LICENSEE AGAINST SUCH CLAIMS UNDER THE INDUSTRIAL INSURANCE PROVISIONS OF TITLE 51 RCW OR ANY APPLICABLE INDUSTRIAL INSURANCE, DISABILITY, OR EMPLOYEE BENEFIT ACT OF ANY JURISDICTION THAT WOULD BE APPLICABLE IN CASE OF SUCH A CLAIM.

In addition to all other indemnities provided in this License, Licensee agrees to protect, defend, and indemnify and hold the Indemnified Parties harmless for any Claims associated with the presence, removal, or remediation of any Hazardous Substance (including petroleum and gasoline products) that are released onto or from the License Area, or otherwise come to be located on the License Area as a result of Licensee's or its Contractors' use of the License Area, including the construction, reconstruction, alteration, maintenance, operation, repair, removal, or relocation of Licensee's Facilities, whether such Claims are made, commenced, or incurred during the term of this Agreement or after the expiration or termination of this Agreement as a result of events occurring during the term of this License. "Hazardous substances," for purposes of this section, include, but are not limited to, those substances included within the definition of "hazardous substances," "hazardous materials," "toxic substances," "hazardous wastes" or solid wastes in any federal, state, or local law, statute, ordinance, regulation, order, or rule pertaining to health, industrial hygiene, environmental conditions, or hazardous substances. "Costs" shall include, but not be limited to, all response or remediation costs, disposal fees, investigation costs, monitoring costs, civil or criminal penalties, attorney's fees, and other litigation costs incurred in connection with such response or remediation.

This indemnification section shall survive the expiration or earlier termination of this License.

13. Insurance.
  - a. Licensee shall procure and maintain, and shall require its Contractors to procure and maintain, at their respective expense, applicable to all activities of Licensee and its Contractors during the Term of this License, and provide proof to Sound Transit that Licensee and its Contractors have secured, the following insurance policies or their equivalent by means of self insurance:
    - i. Commercial Liability insurance in amounts of not less than a combined single limit of \$2,000,000 or in such other amounts as Sound Transit may from time to time reasonably require, insuring Licensee and its officers, directors, agents, and employees; Licensee's Contractors and their officers, directors, agents, and employees; Sound Transit and its officers, directors, agents, and employees; and the County and its officers, directors, agents, and employees against all liability for injury to or death of a person or persons or damage to property arising from the use and occupancy of the License Area, and including coverage for



Bodily Injury and Property Damage Liability, Personal Injury Liability, and containing endorsements covering Contractual Liability, Fire Legal Liability, and Stop-Gap coverage endorsements sufficient to cover Licensee's indemnity obligations hereunder.

- ii. Automobile Liability insurance in amounts of not less than a combined single limit of \$1,000,000 covering Licensee's and its Contractors' owned, non-owned, leased or rented vehicles and naming Sound Transit and its officers, directors, agents, and employees and the County and its officers, directors, agents, and employees as additional insureds.
- iii. All-risk Property insurance covering the full value of Licensee's and its Contractors' property and improvements (including all initial improvements), and other property (including property of others), in the License Area.
- iv. Workers' Compensation and Employers' Liability in accordance with the provisions of Title 51 of the Revised Code of Washington and covering Licensee's employees' industrial accidents and injuries.
- v. Railroad Protective Liability coverage either by: (1) endorsing the General Liability policy with an ISO form CG 24 27 10 01 – Contractual Liability – Railroads endorsement, of equivalent; or (2) obtaining a separate Railroad Protective Liability policy. This insurance shall name Sound Transit and its officers, directors, agents, and employees and the County and its officers, directors, agents, and employees as additional insureds with coverage of at least \$2,000,000 per occurrence and \$6,000,000 in the aggregate. The policy shall be issued on a standard ISO form CG 00 35 10 93, or equivalent.
- c. Unless approved by Sound Transit in advance and in writing, the insurance coverages required herein shall not be subject to any deductible or self-insured retentions of liability greater than \$25,000 per occurrence. The payment of any such deductible or self-insured retention of liability amounts remains the sole responsibility of Licensee.
- d. Prior to entering the License Area, Licensee shall furnish Sound Transit with certificates of insurance executed by a duly authorized representative of each insurer or, if Licensee is self-insuring pursuant to subsection (i), below, other acceptable evidence of insurance as determined by Sound Transit, in its sole discretion, showing compliance with the insurance requirements set forth above and naming Sound Transit and the County as additional insureds, consistent with above-described requirements. Insurance coverage shall be primary with respect to Sound Transit and the County, and any other insurance maintained by Sound Transit and the County shall be excess and not contributing. Licensee and its Contractors and their respective insurers shall require that the applicable insurance policies be endorsed to waive their right of subrogation against Sound Transit and the County. A copy of the Additional Insured Endorsements, Primary and Non-Contributory Endorsements and Waiver of Subrogation Endorsements for both General Liability and Auto Liability must be attached to the required Certificates of Insurance.
- e. Licensee may otherwise meet the insurance requirements herein if Sound Transit determines, in its sole discretion, that Licensee's financial condition is adequate to meet the insurance obligations contained in this Section. If Sound Transit approves Licensee's self insured status for purposes of this License, Licensee shall submit a letter signed by a corporate officer stating that Licensee is a qualified self-insurer, and setting forth the limits of any policy of excess insurance.
- f. Sound Transit reserves the right to reasonably modify the required insurance coverage to reflect the then-current risk management practices and underwriting practices in the insurance industry.

14. Federal Interest.

- a. Notwithstanding anything to the contrary contained in this License, Licensee acknowledges that Sound Transit is the recipient of Federal Transit Administration (FTA) grants which, in part, funded the Project. Licensee further acknowledges that pursuant to FTA grant requirements, Sound Transit must demonstrate and retain “satisfactory continuing control” over the use of the Project property, which is defined as the legal assurance that FTA-funded property will remain available to be used for its originally authorized purpose throughout its useful life or until disposition. Licensee agrees that it will not exercise any right under this License in a manner that compromises or otherwise diminishes the federal interest in the Project property or Sound Transit’s satisfactory continuing control over Project property.
  - b. Licensee further acknowledges that Sound Transit must comply with all applicable federal statutes, regulations, orders, certification and assurances, or other federal law including, but not limited to, those set forth in the current FTA Master Agreement governing transit projects supported with federal assistance awarded through the FTA.
15. Liens. Licensee shall pay for all materials and labor used in the License Area and shall not allow any liens to attach to the License Area. In the event that the License Area becomes subject to any lien arising out of Licensee’s use of the License Area under this License, Licensee shall promptly, and in any event within 30 days, cause such lien to be discharged or released of record (by payment, posting of bond, court deposit, or other means), without cost to Sound Transit, and shall indemnify Sound Transit against all costs and expenses (including attorney’s fees) incurred in discharging and releasing such lien. If any such lien is not so discharged and released, Sound Transit may pay or secure the release or discharge thereof at Licensee’s expense.
  16. Attorney’s Fees. In the event that either Party commence litigation or arbitration proceedings against the other Party relating to the performance or alleged breach of this License, the prevailing Party shall be entitled to all costs, including reasonable attorney’s fees incurred, relating to such litigation, including those incurred in the event of any appeal.
  17. Jurisdiction and Venue. Any litigation filed by either Party arising out of or relating to this Agreement shall be filed in King County Superior Court, except as to matters that are exclusively within the jurisdiction of the federal courts and, as to such matters, venue shall be in the Western District of the United States District Court at Seattle, Washington.
  18. Notice. Unless expressly otherwise agreed between the Parties, every notice or response required by this Agreement to be served upon Sound Transit or Licensee shall be in writing and shall be deemed to have been duly given to the required Party: (a) five business days after being posted in a properly sealed and correctly addressed envelope when sent by mail, postage prepaid; (b) upon receipt when sent by overnight delivery through a nationally recognized courier service that provides a receipt of delivery; (c) upon receipt when hand delivered; or (d) upon delivery when sent by email with delivery receipts enabled. The notices or responses to Sound Transit shall be addressed as follows:

Sound Transit  
Union Station  
401 S. Jackson Street  
Seattle, WA 98104  
Attn: Property Management Manager  
propertymanagement@soundtransit.org

The notices or responses to Licensee shall be addressed as follows:

AMLI Development Co  
425 Pontius Ave N #400  
Seattle, WA 98109

Attn: Matt Ellay  
melley@amli.com

Either Party may, from time to time, replace the notice addresses contained herein with alternative notice addresses by giving written notice of same to the other Party, but in no event shall notice be required to be delivered to more than one address.

19. Assignment. This Agreement and the rights, duties and obligations given hereunder may not be assigned, transferred, or otherwise conveyed by Licensee
20. Reservation of Rights. Nothing in this Agreement is intended to modify the provisions of the Transit Way Agreement or to in any way limit Sound Transit's rights thereunder.

Please acknowledge your agreement with the foregoing terms by signing in the space provided below and returning one copy of this Agreement to my attention.

Sincerely,

Clint Dameron  
Acting: Director-Real Property

**ACCEPTED AND AGREED:**  
PPF AMLI Redmond Way, LLC

By: \_\_\_\_\_

Its: \_\_\_\_\_

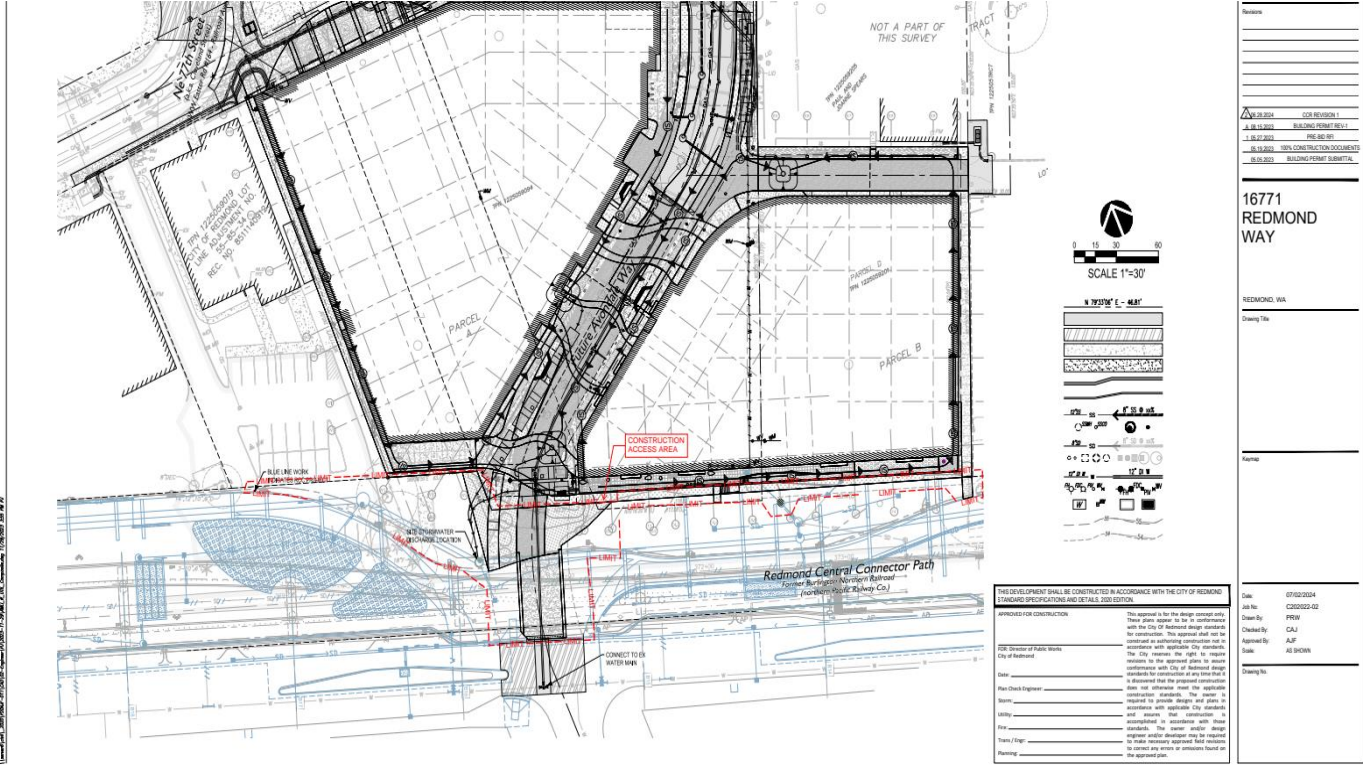
Date: \_\_\_\_\_

Approved as to form:

\_\_\_\_\_

Sound Transit Legal Counsel

EXHIBIT A  
LICENSE AREA







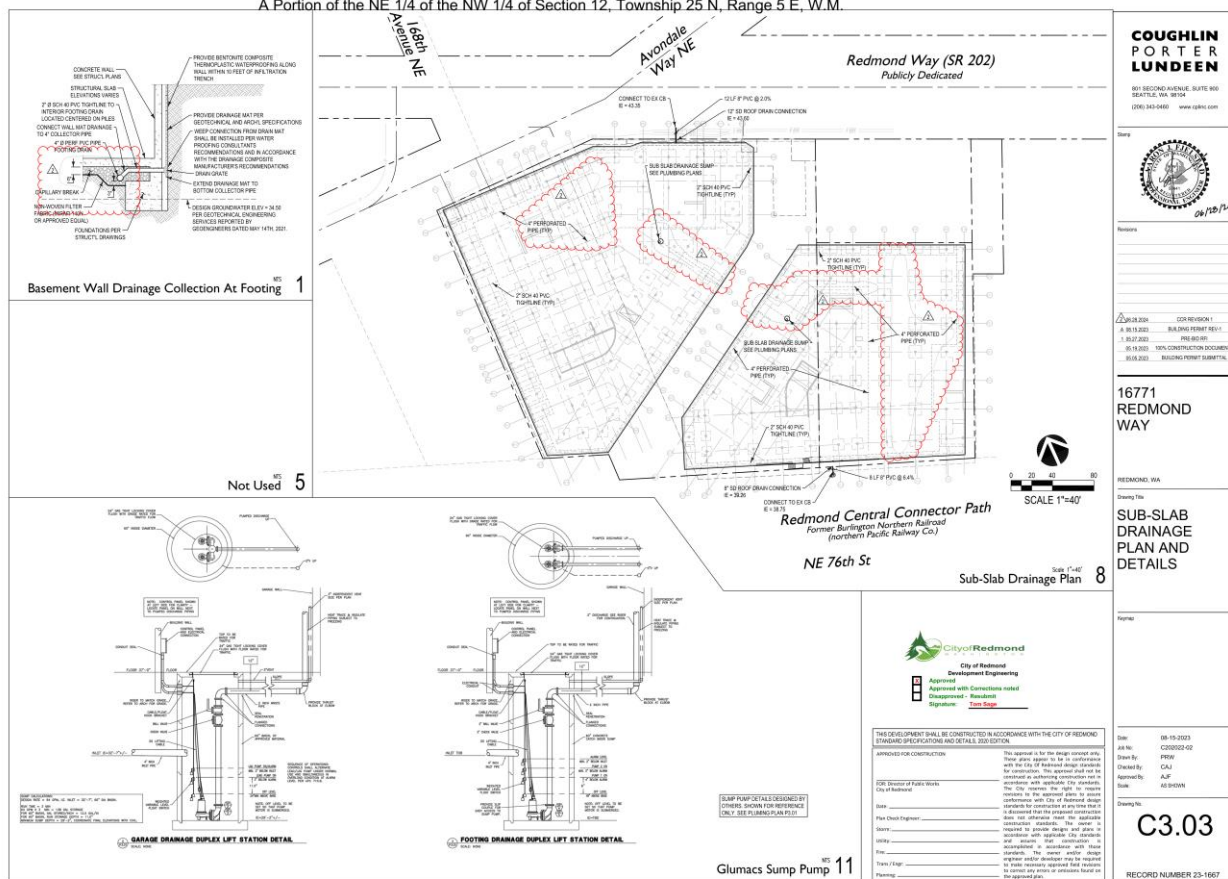
11

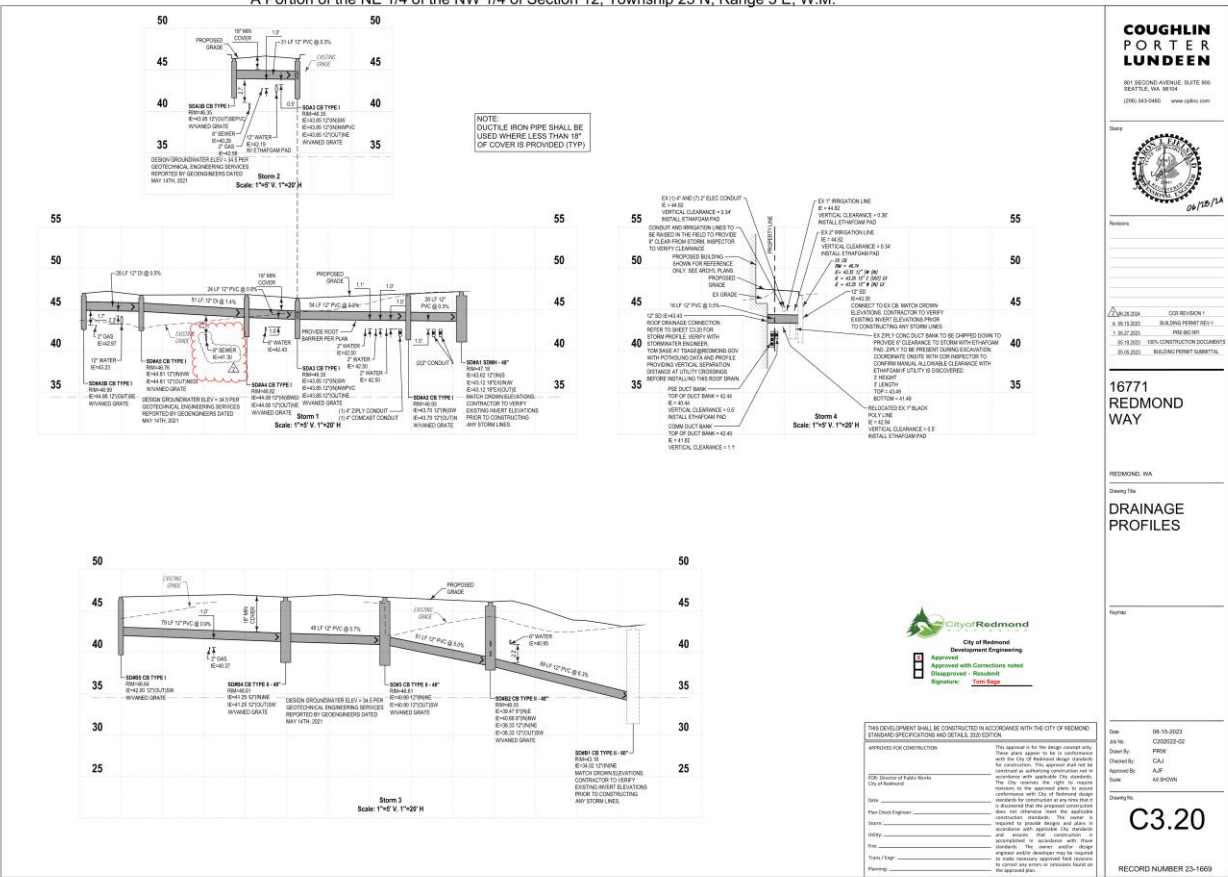







A Portion of the NE 1/4 of the NW 1/4 of Section 12, Township 25 N., Range 5 E., W.M.







<b>COUGHLIN PORTER LUNDEEN</b>	
801 SECOND AVENUE SUITE 800 SEATTLE, WA 98104 (206) 443-0400    www.cplaw.com	
Library	 <div style="position: absolute; top: 0; right: 0; transform: rotate(45deg); font-weight: bold;">04/15/17</div>
Revisions	
<div style="display: flex; justify-content: space-between;"> <span>△ 3-16-2018 CORRECTIONS</span> <span>3-16-2018 BUILDING PERMIT #001 -</span> </div> <div style="display: flex; justify-content: space-between;"> <span>1-30-2017 PERMITS</span> <span>PERMITS</span> </div> <div style="display: flex; justify-content: space-between;"> <span>10-11-2013 NON CONSTRUCTION DOCUMENTS</span> <span>NON CONSTRUCTION DOCUMENTS</span> </div> <div style="display: flex; justify-content: space-between;"> <span>3-16-2018 BUILDING PERMIT SUBMITTAL</span> <span>BUILDING PERMIT SUBMITTAL</span> </div>	
<b>16771 REDMOND WAY</b>	
REDMOND, WA	
<b>UTILITY PLAN - WEST</b>	
Drawing Title	
Project:	
Date:	08-15-2023
As Issued:	CSD2023-02
Drawn By:	FHW
Checked By:	CHJ
Inscribed By:	AJP
Scaled:	AS SHOWN
Drawing No.	

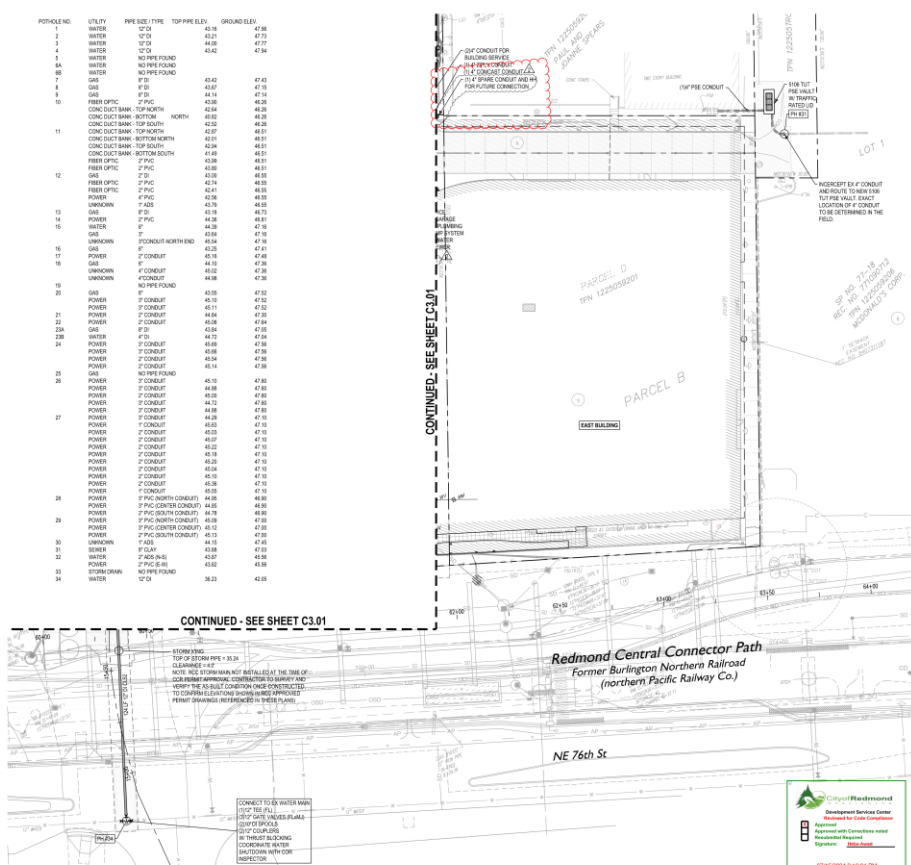
C4.01

RECORD NUMBER 23-167



A Portion of the NE 1/4 of the NW 1/4 of Section 12, Township 25 N, Range 5 E, W.M.

PROBE NUMBER	UTILITY	PIPE DEPTH (FT)	TOP PIPE LEVEL	GROUND LEVEL
1	WATER	17'0"	43.16	44.00
2	WATER	17'0"	43.21	44.00
3	WATER	17'0"	43.27	44.00
4	WATER	17'0"	43.42	44.00
5	WATER	17'0"	43.47	44.00
6A	WATER	NO PIPE FOUND		
7	GAS	8'0"	43.43	44.00
7	GAS	8'0"	43.43	44.00
8	GAS	8'0"	43.67	44.00
9	FIBER OPTIC	2'0"	43.67	44.00
10	CONDUIT BANK, TOP NORTH		43.64	44.00
11	CONDUIT BANK, TOP NORTH		43.64	44.00
11	CONDUIT BANK, TOP SOUTH		43.82	44.00
12	CONDUIT BANK, TOP SOUTH		43.82	44.00
12	CONDUIT BANK, TOP SOUTH		43.84	44.00
12	CONDUIT BANK, TOP SOUTH		43.84	44.00
12	FIBER OPTIC	2'0"	43.98	44.00
12	FIBER OPTIC	2'0"	43.98	44.00
12	GAS	2'0"	44.01	44.00
12	GAS	2'0"	44.01	44.00
13	FIBER OPTIC	2'0"	44.21	44.00
13	FIBER OPTIC	2'0"	44.21	44.00
13	UNKNOWIN	7'0"	44.21	44.00
13	UNKNOWIN	7'0"	44.21	44.00
14	POWER	2'0"	44.38	44.00
15	POWER	2'0"	44.38	44.00
16	UNKNOWIN	7'0"	44.38	44.00
16	UNKNOWIN	7'0"	44.38	44.00
17	POWER	2'0"	44.59	44.00
17	POWER	2'0"	44.59	44.00
18	UNKNOWIN	7'0"	44.59	44.00
18	UNKNOWIN	7'0"	44.59	44.00
19	GAS	NO PIPE FOUND		
20	GAS	2'0"	43.55	44.00
20	POWER	2'0"	43.55	44.00
21	POWER	2'0"	43.55	44.00
21	POWER	2'0"	43.55	44.00
22	POWER	2'0"	43.71	44.00
22	POWER	2'0"	43.71	44.00
23B	WATER	6'0"	44.72	44.00
23B	WATER	6'0"	44.72	44.00
24	POWER	2'0"	44.72	44.00
24	POWER	2'0"	44.72	44.00
25	POWER	2'0"	44.84	44.00
25	POWER	2'0"	44.84	44.00
26	POWER	2'0"	45.11	44.00
26	POWER	2'0"	45.11	44.00
27	POWER	2'0"	45.02	44.00
27	POWER	2'0"	45.02	44.00
28	POWER	2'0"	45.12	44.00
28	POWER	2'0"	45.12	44.00
29	POWER	2'0"	45.03	44.00
29	POWER	2'0"	45.03	44.00
30	POWER	2'0"	45.07	44.00
30	POWER	2'0"	45.07	44.00
31	POWER	2'0"	45.20	44.00
31	POWER	2'0"	45.20	44.00
32	POWER	2'0"	45.21	44.00
32	POWER	2'0"	45.21	44.00
33	POWER	2'0"	45.36	44.00
33	POWER	2'0"	45.36	44.00
34	POWER	2'0"	45.05	44.00
34	POWER	2'0"	45.05	44.00
35	POWER	2'0"	45.12	44.00
35	POWER	2'0"	45.12	44.00
36	POWER	2'0"	45.17	44.00
36	POWER	2'0"	45.17	44.00
37	POWER	2'0"	45.03	44.00
37	POWER	2'0"	45.03	44.00
38	POWER	2'0"	45.17	44.00
38	POWER	2'0"	45.17	44.00
39	UNKNOWIN	7'0"	44.12	44.00
39	UNKNOWIN	7'0"	44.12	44.00
40	WATER	8'0"	43.67	44.00
40	WATER	8'0"	43.67	44.00
41	STORM DRAIN	NO PIPE FOUND		
41	STORM DRAIN	NO PIPE FOUND		



Call before you  
Dig. 8-1-1  
or 1-800-424-3333  
UNDERGROUND  
SERVICE (USA)

**COUGHLIN  
PORTER  
LUNDEEN**

601 SECOND AVENUE, SUITE 900  
SEATTLE, WA 98104  
(206) 343-0460 [www.cplinc.com](http://www.cplinc.com)



30.29.2024	CDR REVISION 1
30.15.2023	BUILDING PERMIT REV-1
30.27.2023	PRE-BID RFI
05.19.2023	100% CONSTRUCTION DOCUMENTS
05.06.2023	BUILDING PERMIT SUBMITTAL

16771  
REDMOND  
WAY

REDMOND, WA

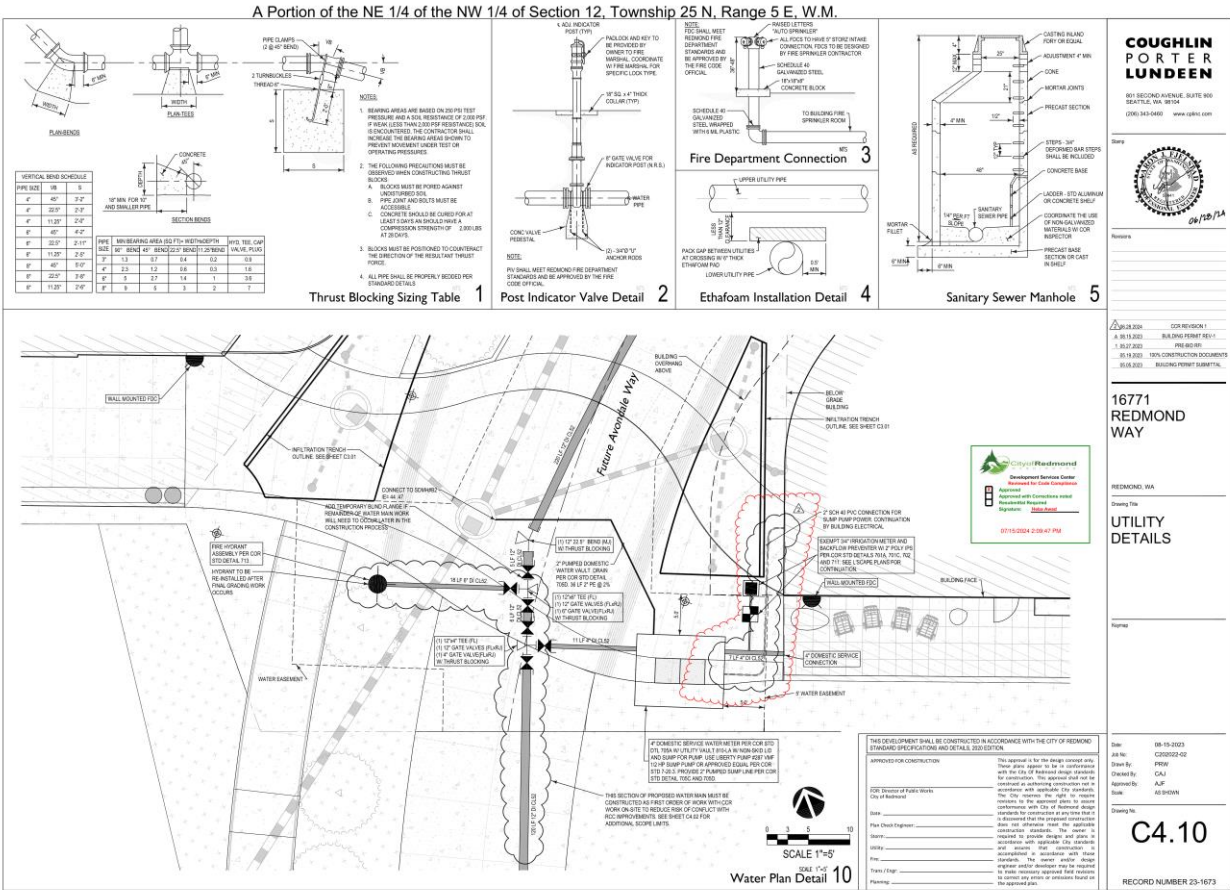
UTILITY PLAN -  
EAST

Raymond

Date: 08-15-2023  
Job No: C202022-02  
Drawn By: PFEW  
Checked By: CAJ  
Approved By: AJF  
Scale: AS SHOWN

C4.02

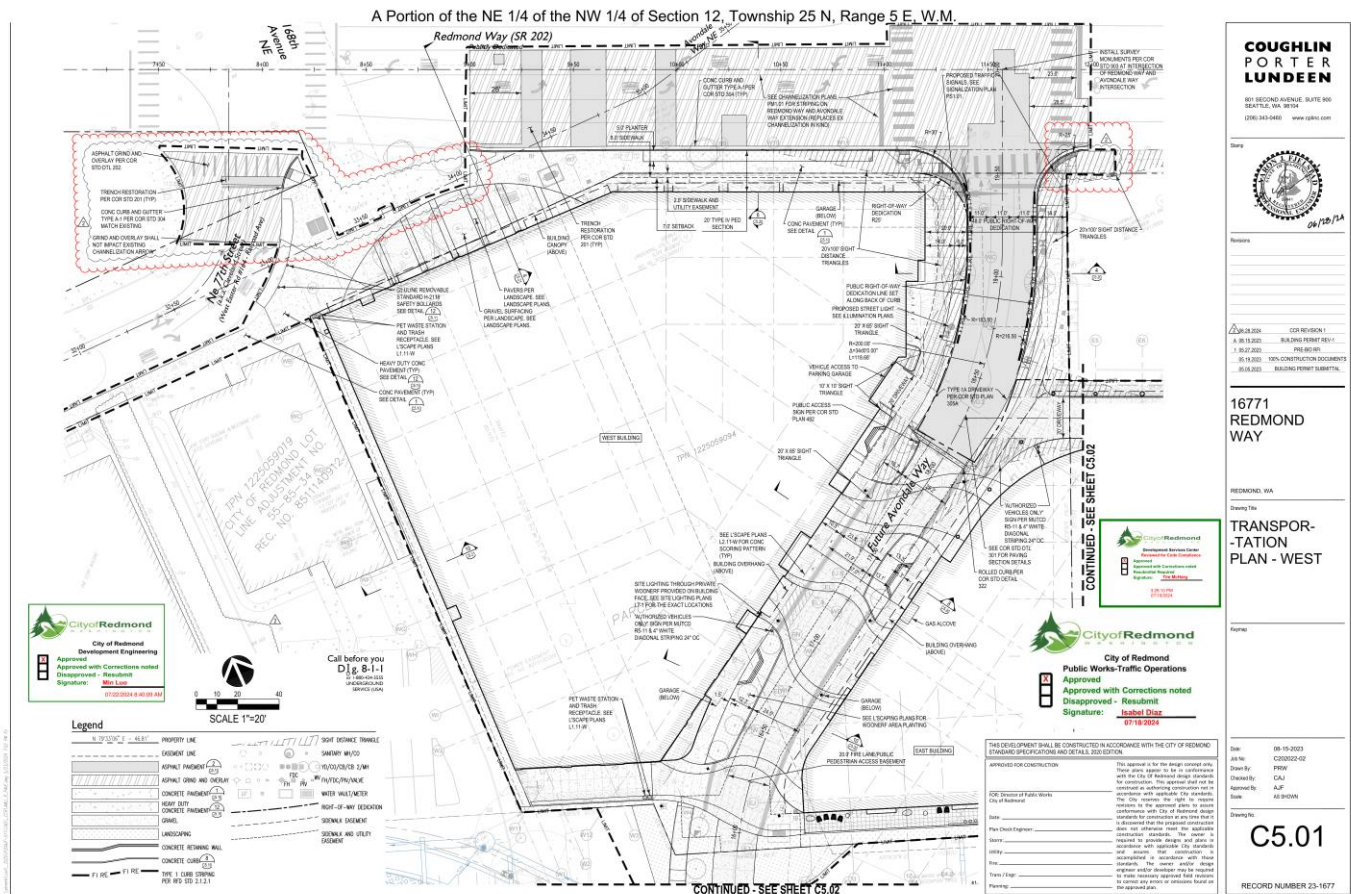
RECORD NUMBER 23-1672



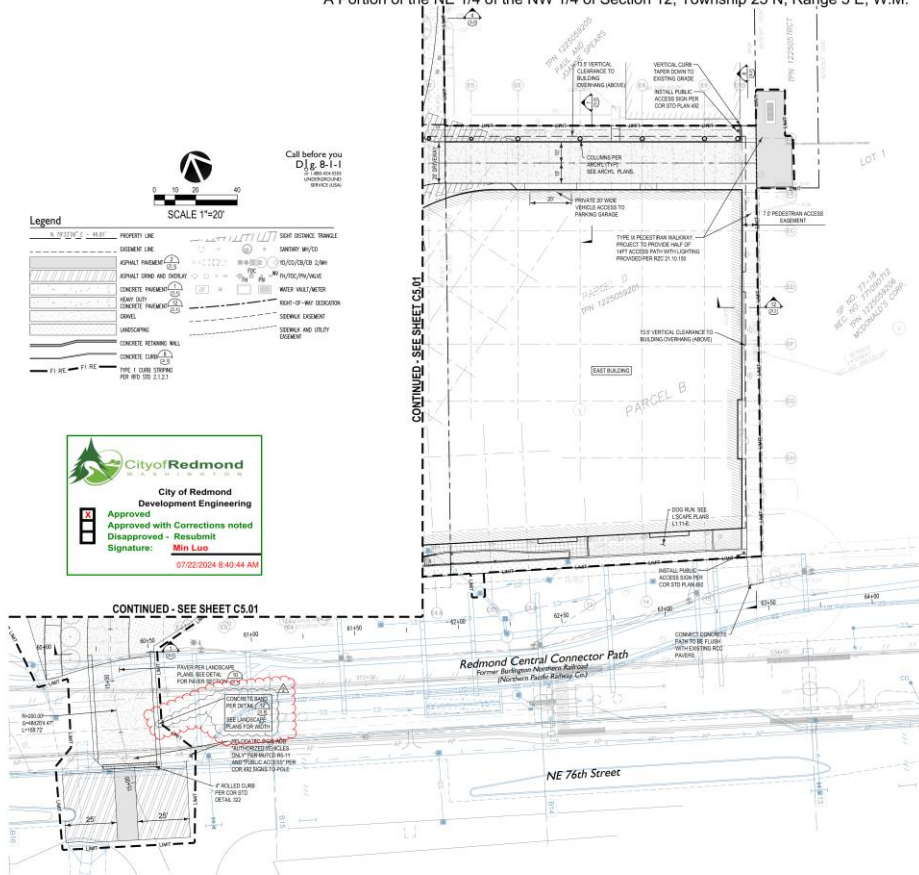











A Portion of the NE 1/4 of the NW 1/4 of Section 12, Township 25 N, Range 5 E, W.M.



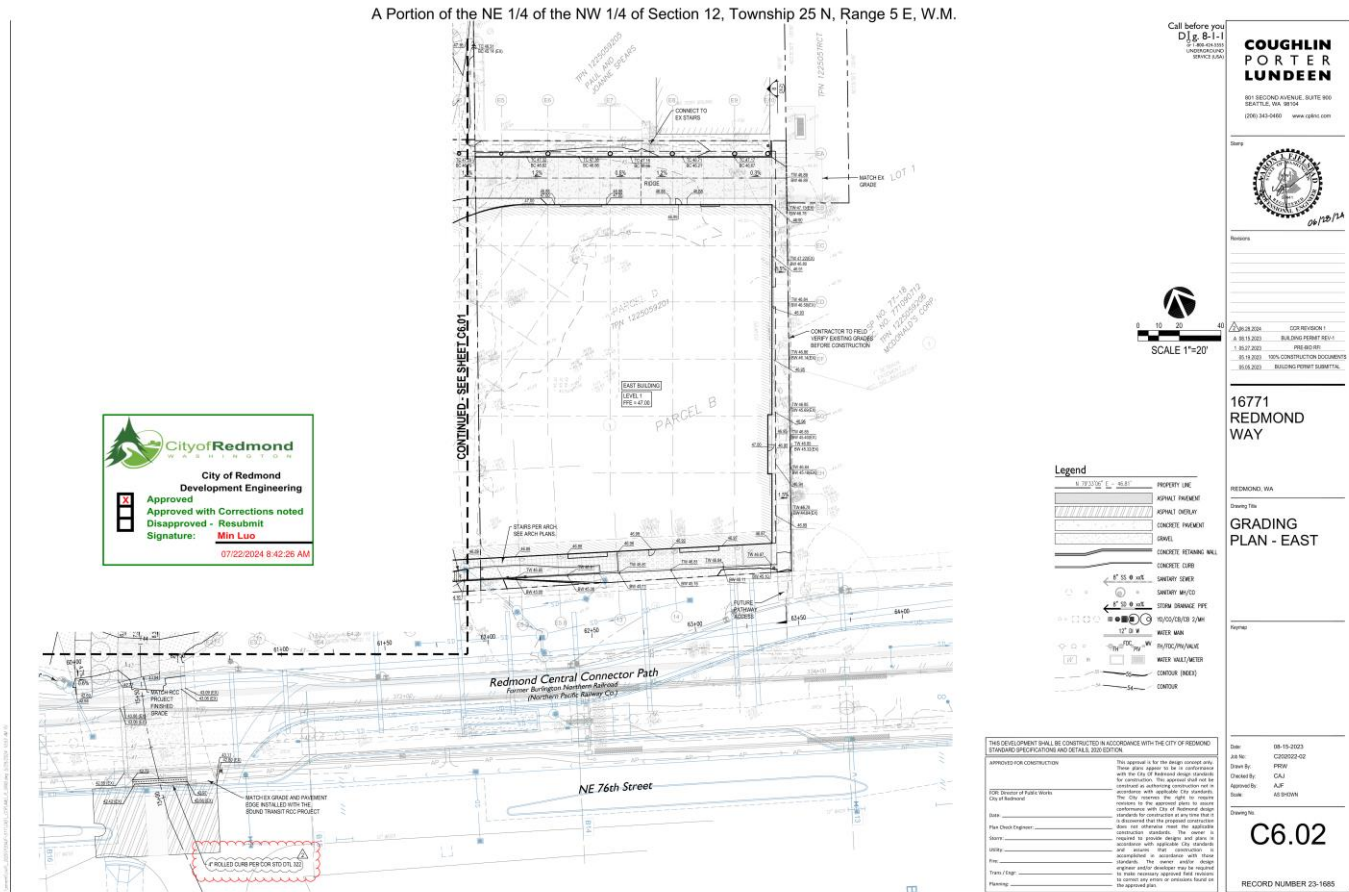
<b>COUGHLIN PORTER LUNDEEN</b>	
851 SECOND AVENUE, SUITE 900 SEATTLE, WA 98104 (206) 343-0400      www.cplaw.com	
Date:	<div style="text-align: center;">            06/12/15A       </div>
Signature:	<div style="border-bottom: 1px solid black; height: 20px; width: 100%;"></div>
<div>          JULIA 2024      CDR REVISION 1       </div>	<div>          JULIA 2024      JULIA 2024 REVISION 1       </div>
1. JULY 2024      PMS 80-001	1. JULY 2024      PMS 80-001
2. JULY 2024      100% CONSTRUCTION DOCUMENTS	2. JULY 2024      100% CONSTRUCTION DOCUMENTS
3. JULY 2024      100% CONSTRUCTION SUBMITTALS	3. JULY 2024      100% CONSTRUCTION SUBMITTALS
<h1 style="margin: 0;">16771 REDMOND WAY</h1>	
REDMOND, WA	
Drawing Title:	
<h2 style="margin: 0;">TRANSPOR- TATION PLAN - EAST</h2>	
Number:	
Date:	06-10-2023
Job No:	CDR002-002
Drawn By:	PMSW
Checked By:	CMJ
Approved By:	ALP
Scale:	AS SHOWN
Drawing No:	
<h1 style="margin: 0;">C5.02</h1>	
RECORD NUMBER 23-1678	













THIS DEVELOPMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF REMOND STANDARD SPECIFICATIONS AND DETAILS, 2008 EDITION.

APPROVED FOR CONSTRUCTION

This approval is the design concept only. These plans appear to be in conformance with the City of Remond standards for construction. This approval shall not be construed as an endorsement or warranty in accordance with applicable City standards, specifications, and policies. The engineer assumes no responsibility for the compliance of the submitted plans to assure performance by City of Remond standards for construction at any time that it is determined that the design is not in accordance with applicable City standards. The engineer does not otherwise make the applicable City standards a part of these plans.

The engineer assumes no responsibility required to provide designs and plans in accordance with applicable City standards and assume that construction is accomplished in accordance with these standards. The owner, owner's engineer and/or developer may be required to make reasonable independent field inspections to ensure any errors or omissions found on the approved plans.

FOR Director of Public Works  
City of Remond

Date \_\_\_\_\_

Print Name/Engineer \_\_\_\_\_

State \_\_\_\_\_

City \_\_\_\_\_

Office \_\_\_\_\_

Firm \_\_\_\_\_

Print Name/Engineer \_\_\_\_\_

Planning \_\_\_\_\_

**COUGHLIN  
PORTER  
LUNDEEN**

601 SECOND AVENUE, SUITE 900  
SEATTLE, WA 98104  
(206) 343-0460 [www.cadent.com](http://www.cadent.com)

[illegible]

16771  
REDMOND  
WAY

REDMOND, WA  
Drawing Title  
**GRADING  
PLAN - DETAIL**

Date:	08-15-2023
Job No:	C200022-02
Drawn By:	PRFW
Checked By:	CAL
Approved By:	AJF
Scale:	AS SHOWN

C6.03

RECORD NUMBER 23-1680

**Grading Details**

**Redmond Way (SR 202)**

**Redmond Way (SR 207)**

**ADA Ramp Grading Detail 1**

**ADA Ramp Grading Detail 2**

**ADA Ramp Grading Detail 3**

**ADA Ramp Grading Detail 4**

**Not Used 1**

**Not Used 2**

**Not Used 3**

**Not Used 4**

**Not Used 5**

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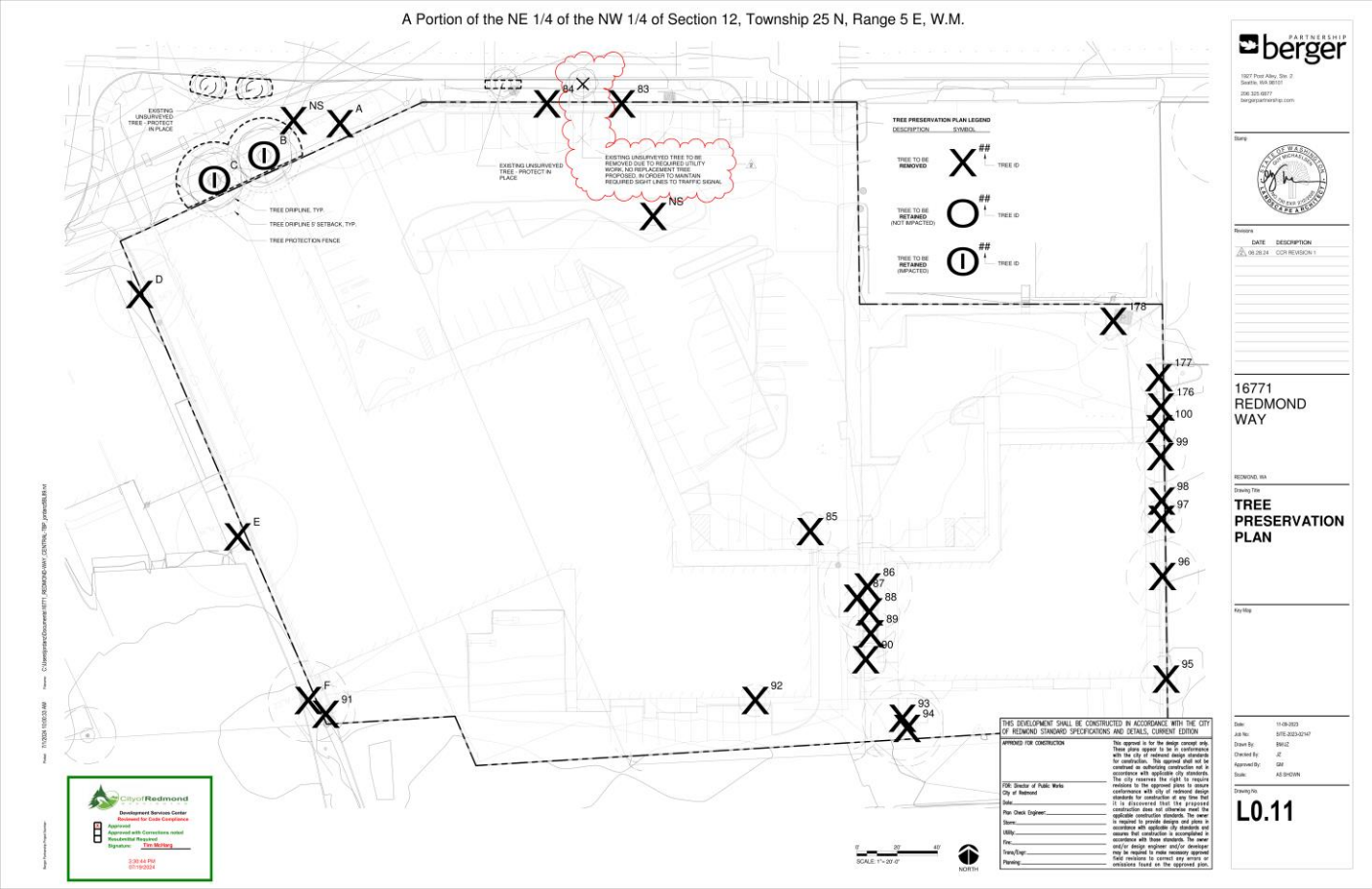
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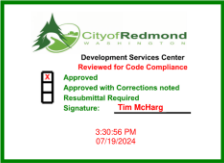




A Portion of the NE 1/4 of the NW 1/4 of Section 12, Township 25 N, Range 5 E, W.M.

LANDSCAPE ZONES - GROUND LEVEL		TOTAL PLANTING AREAS PER LANDSCAPE ZONE	
Zone Name	TOTAL AREA	Zone	TOTAL PLANTING AREA
EAST BUILDING ENTRY	474 SF	EAST BUILDING ENTRY	461 SF
EAST BUILDING FRONTAGE	1566 SF	EAST BUILDING FRONTAGE	1352 SF
EAST BUILDING SOUTH SETBACK	3702 SF	EAST BUILDING SOUTH SETBACK	1895 SF
PEDESTRIAN WALKWAY	120 SF	PEDESTRIAN WALKWAY	114 SF
SERVICE AREA	4557 SF	SERVICE AREA	1037 SF
SIDEWALK AREA	12906 SF	WEST BUILDING ENTRY	129 SF
WEST BUILDING ENTRY	254 SF	WEST BUILDING FRONTAGE	1821 SF
WEST BUILDING FRONTAGE	2065 SF	WEST BUILDING SIDE SETBACK	1408 SF
WEST BUILDING SIDE SETBACK	1289 SF	WEST BUILDING SOUTH SETBACK	284 SF
WEST BUILDING SOUTH SETBACK	1254 SF	Grand total: 27	8502 SF
Grand total: 28	28187 SF		

LANDSCAPE ZONES - UPPER LEVELS		TOTAL PLANTING AREAS PER LANDSCAPE ZONE - UPPER LEVELS	
Zone Name	TOTAL AREA	Zone	TOTAL PLANTING AREA
EAST BUILDING - FAMILY DECK	7013 SF	EAST BUILDING - FAMILY DECK	595 SF
EAST BUILDING - L2 PLAY COURTYARD	8430 SF	EAST BUILDING - PLAY COURTYARD	1533 SF
EAST BUILDING - SOLARIUM DECK	496 SF	EAST BUILDING - SOLARIUM DECK	151 SF
WEST BUILDING - L2 CLUBHOUSE COURTYARD	6075 SF	WEST BUILDING - CLUBHOUSE COURTYARD	1400 SF
WEST BUILDING - L2 WEST COURTYARD	5228 SF	WEST BUILDING - L2 WEST COURTYARD	2567 SF
WEST BUILDING - TREEHOUSE DECK	5294 SF	WEST BUILDING - TREEHOUSE DECK	619 SF
WEST BUILDING PET AREA	766 SF	Grand total: 43	6865 SF
Grand total: 7	33301 SF		



LANDSCAPE AND TREE AREAS CODE COMPLIANCE			
LANDSCAPE AREA		28,334 SF (PER ARCH CALCULATIONS A0.31)	
COMMON OPEN SPACE REQUIRED		13,167 SF (28,334 x 50%)	
LANDSCAPE (PREVIOUS) TO MAKE UP 50% MIN OF REQUIRED OPEN SPACE:		13,167 SF (28,334 x 50%)	
PROVIDED LANDSCAPE AREA GRAND TOTAL (AREAS SHOWN ABOVE):		8,552 + 4,665 = 13,217 SF	
TREE REPLACEMENT			
ON SITE SIGNIFICANT TREES REMOVED UP TO 35%	SIGNIFICANT	REPLACEMENT RATIO	REPLACEMENT REQUIRED
ON SITE SIGNIFICANT TREES REMOVED OVER 35%	11	1:1	11
ON SITE TOTAL REPLACEMENT REQUIRED	8	3:1	24
OFF SITE ROW TREES REMOVED	1	1:1	1
OFF SITE ROW TREES IMPACTED	2	1:1	2
OFF SITE TOTAL REPLACEMENT REQUIRED			29
REFER TO ARBORIST REPORT FOR ADDITIONAL INFORMATION ON TREE IMPACTS			
NOTE: OFF SITE ROW TREE QUANTITIES ABOVE EXCLUDES TREES THAT WERE NOT IDENTIFIED IN SURVEY OR ARBORIST REPORT DUE TO THEIR SMALL SIZE. THERE IS ONE EXISTING UNSURVEYED ROW TREE ON REDMOND WAY THAT WILL NEED TO BE REMOVED AND REPLACED DUE TO ADJACENT UTILITY WORK. REFER TO L0.11 FOR LOCATION AND L4.11-W FOR REPLACEMENT.			
ON SITE NEW TREES PROPOSED	18	10 (INCLUDES NEW TREES IN POTENTIAL FUTURE AVONDALE WAY)	
OFF SITE NEW TREES PROPOSED			

THE DEVELOPMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF REDMOND STANDARD SPECIFICATIONS AND DETAILS, CURRENT EDITION	
APPROVED FOR CONSTRUCTION	The approved is for the design concept only. These plans are subject to review and approval for construction. This approval shall not be construed as endorsing construction and is not a guarantee of the quality of the construction. The City of Redmond is not responsible for the design or construction of the project. The City of Redmond is not responsible for the design or construction of the project. The City of Redmond is not responsible for the design or construction of the project.
FOR Review of Public Works	City of Redmond
Signature	
Date	
For Owner Approval	
Signature	
Date	
For City Approval	
Signature	
Date	



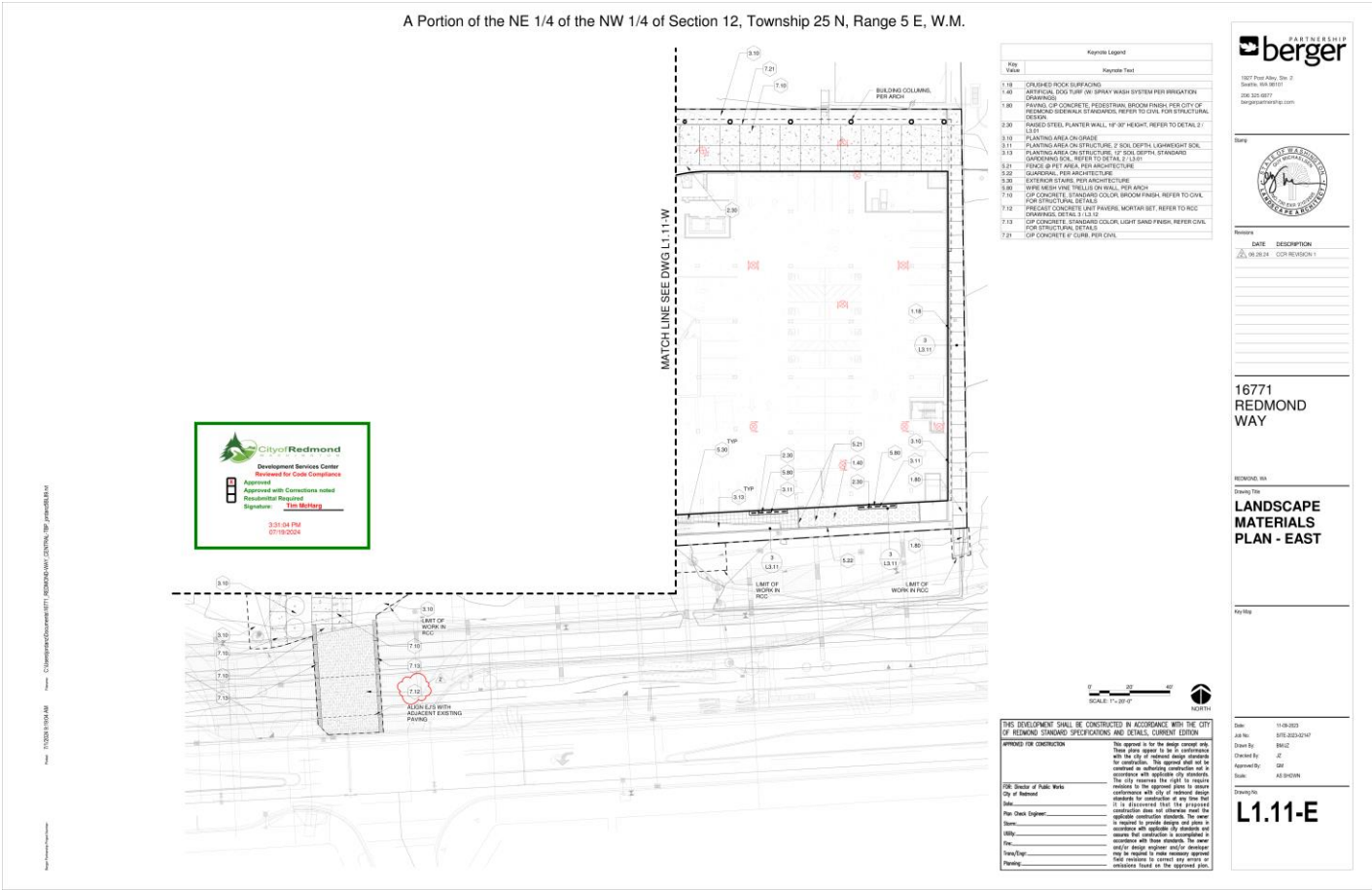
16771  
REDMOND  
WAY

REDEMPTO, WA  
PROJECT NO.  
**CODE COMPLIANCE -  
LANDSCAPE  
QUANTITIES &  
ECO SCORE**

Key Map

Date:	11/09/2023
Job No:	1671-2023-02147
Drawn By:	BM/2
Checked By:	JL
Approved By:	GM
Scale:	AS SHOWN
Drawn By:	

**L0.12**

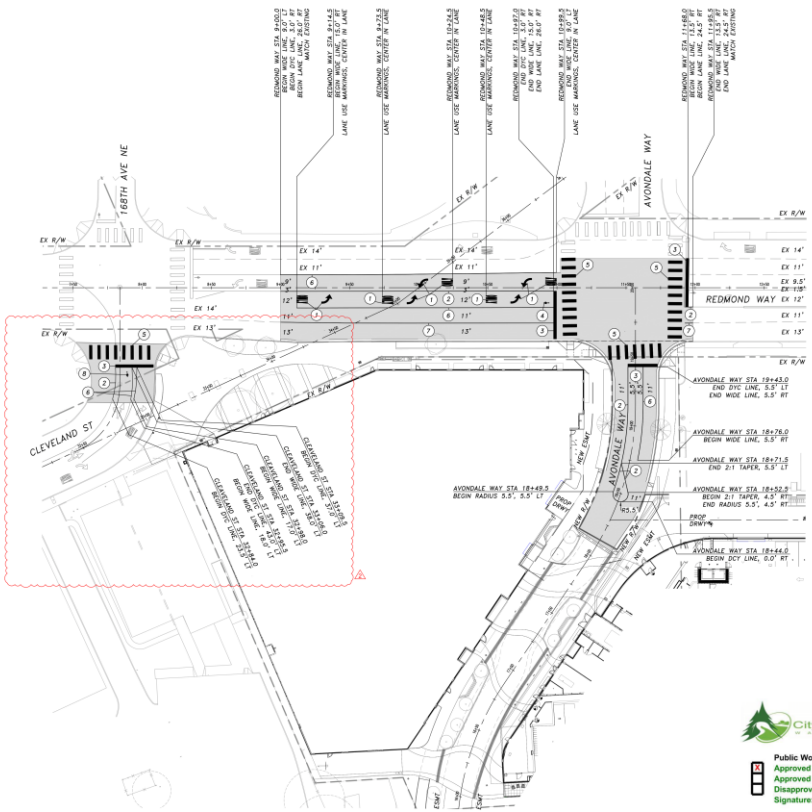








A Portion of the NE 1/4 of the NW 1/4 of Section 12, Township 25 N, Range 5 E, W.M.



PAVEMENT MARKING & SIGNING GENERAL NOTES

- 1. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING ALL SIGNS AND PAVEMENT MARKINGS IN ACCORDANCE WITH THESE PLANS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE CITY OF REDMOND STANDARDS AND THE REFERENCED WSDOT STANDARD PLANS AND SPECIFICATIONS.
- 2. CONTRACTOR SHALL COORDINATE WITH THE CITY INSPECTOR TO APPROVE STRIPING LAYOUT PRIOR TO INSTALLING PERMANENT STRIPING.
- 3. REMOVE EXISTING CONFLICTING STRIPING AS NECESSARY TO ACCOMMODATE NEW STRIPING. CONTRACTOR SHALL COORDINATE STRIPING REMOVE WITH ASPHALT RESTORATION WORK.
- 4. ALL NECESSARY SIGNS AND MARKINGS ON-SITE, ALONG PROPERTY FRONTAGE, AND AT SPECIFICALLY DESIGNATED OFF-SITE LOCATIONS SHALL BE PROVIDED BY THE APPLICANT AS REQUIRED BY THE TRAFFIC OPERATIONS DIVISION WHETHER OR NOT THESE ARE INDICATED ON THE CIVIL CONSTRUCTION DRAWINGS.
- 5. CONTRACTOR SHALL REFRESH ANY PAVEMENT MARKINGS DISTURBED DURING CONSTRUCTION.

PAVEMENT MARKING CONSTRUCTION NOTES

- 1. INSTALL WHITE THERMOPLASTIC LANE-USE PAVEMENT MARKING PER CITY OF REDMOND STANDARD DETAIL 418.
- 2. INSTALL RPW TYPE B DOUBLE YELLOW CENTER (DYC) LINE PER CITY OF REDMOND STANDARD DETAIL 312.
- 3. INSTALL 24" WHITE THERMOPLASTIC STOP BAR PER CITY OF REDMOND STANDARD DETAIL 320A.
- 4. INSTALL 18" WHITE THERMOPLASTIC CROSSWALK BARS PER CITY OF REDMOND STANDARD DETAIL 311B.
- 5. INSTALL RPW TYPE D WHITE WIDE LINE PER CITY OF REDMOND STANDARD DETAIL 312.
- 6. INSTALL RPW TYPE A WHITE LANE LINE PER CITY OF REDMOND STANDARD DETAIL 312.
- 7. INSTALL BICYCLE DETECTOR PAVEMENT MARKING PER CITY OF REDMOND STANDARD DETAIL 320A.



LEGEND	
EXISTING	NEW
DESCRIPTION	DESCRIPTION
PAVEMENT MARKING	PAVEMENT MARKING
PAVEMENT OVERLAY/REPLACEMENT LIMITS	PAVEMENT OVERLAY/REPLACEMENT LIMITS

THIS DEVELOPMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF REDMOND STANDARD SPECIFICATIONS AND DETAILS 2024 EDITION.

APPROVED FOR CONSTRUCTION

The applicant is for the design concept only. These plans are subject to review and approval by the City of Redmond. The City reserves the right to require modifications to the approved plans for reasons of public safety, health, or general welfare. The City reserves the right to require modifications to the approved plans for reasons of public safety, health, or general welfare. The City reserves the right to require modifications to the approved plans for reasons of public safety, health, or general welfare.

Date: \_\_\_\_\_

Per-Check Engineer: \_\_\_\_\_

Drawn By: \_\_\_\_\_

Checked By: \_\_\_\_\_

Reviewed By: \_\_\_\_\_

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

City of Redmond  
Public Works/Traffic Operations  
Approved  
Approved with Corrections noted  
Disapproved - Resubmit  
Signature: Isabel Diaz  
07/16/2024

TENW  
TRANSPORTATION ENGINEERING NORTHWEST  
16771 REDMOND WAY  
REDMOND, WA 98072  
PHONE: 206.881.1234  
FAX: 206.881.1235  
WWW.TENW.COM

06-28-2024

16771  
REDMOND  
WAY

REDMOND, WA

PAVEMENT  
MARKING  
PLAN

Date: 06-28-2024

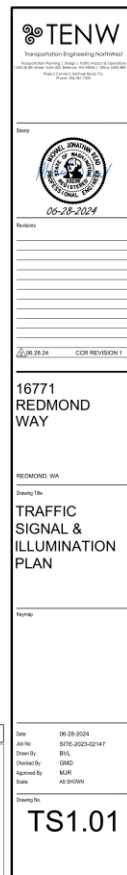
Drawn By: BVL

Checked By: GMS

Reviewed By: MGR

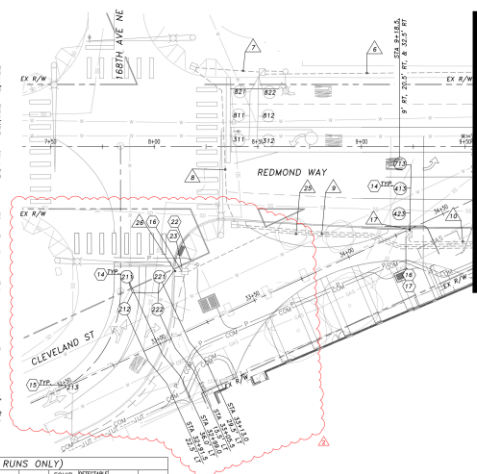
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Drawing No: PM1.01



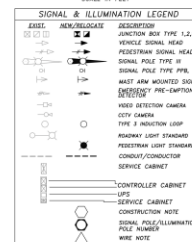


- [illegible]

[illegible]

\* OTHER EXISTING CONDUIT/CONDUCTORS MAY BE PRESENT IN EXISTING CONDUIT.

\*\* ALL NON-METAL CONDUIT CONTAINING COPPER CONDUCTORS SHALL INCLUDE GROUND WIRE  
GROUND WIRE SIZE SHALL MATCH THE LARGEST CONDUCTOR (MIN. 8 AWG)



THIS DEVELOPMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF REDMOND STANDARD SPECIFICATIONS AND DETAILS, 2008 EDITION.

APPROVED FOR CONSTRUCTION:

100

ROB: Director of Public Works  
City of Richmond

Date: \_\_\_\_\_

Plan Check Engineer: \_\_\_\_\_

utility: \_\_\_\_\_

Rec: \_\_\_\_\_

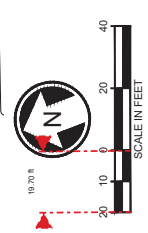
Trans / Eng: \_\_\_\_\_  
Planning: \_\_\_\_\_

10/10/2016



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1. ALL PLANTING AREAS TO RECEIVE SOIL PREP. SEE ENLARGED PLANTING PLANS AND PLANTING ENLARGEMENTS FOR SPECIFIC SOIL PREPARATIONS.
2. SEE E31-LPS100 AND E31-LPS101 FOR PLANT SCHEDULE, HATCH PATTERNS, AND NOTES.
3. SEE E31-LPK100 FOR STATION SHEET KEY LAYOUT.
4. SEE E31-LZN001 FOR PLANT MATERIAL SETBACK CHART AND GENERAL NOTES.
5. RCC TRAIL PLANTING CONTINUATION IS INCLUDED IN THE HEAVY CIVIL GUIDE WAY PACKAGE ON L30-LPP DRAWINGS.



DOWNTOWN STATION PLANTING DESIGN- EAST

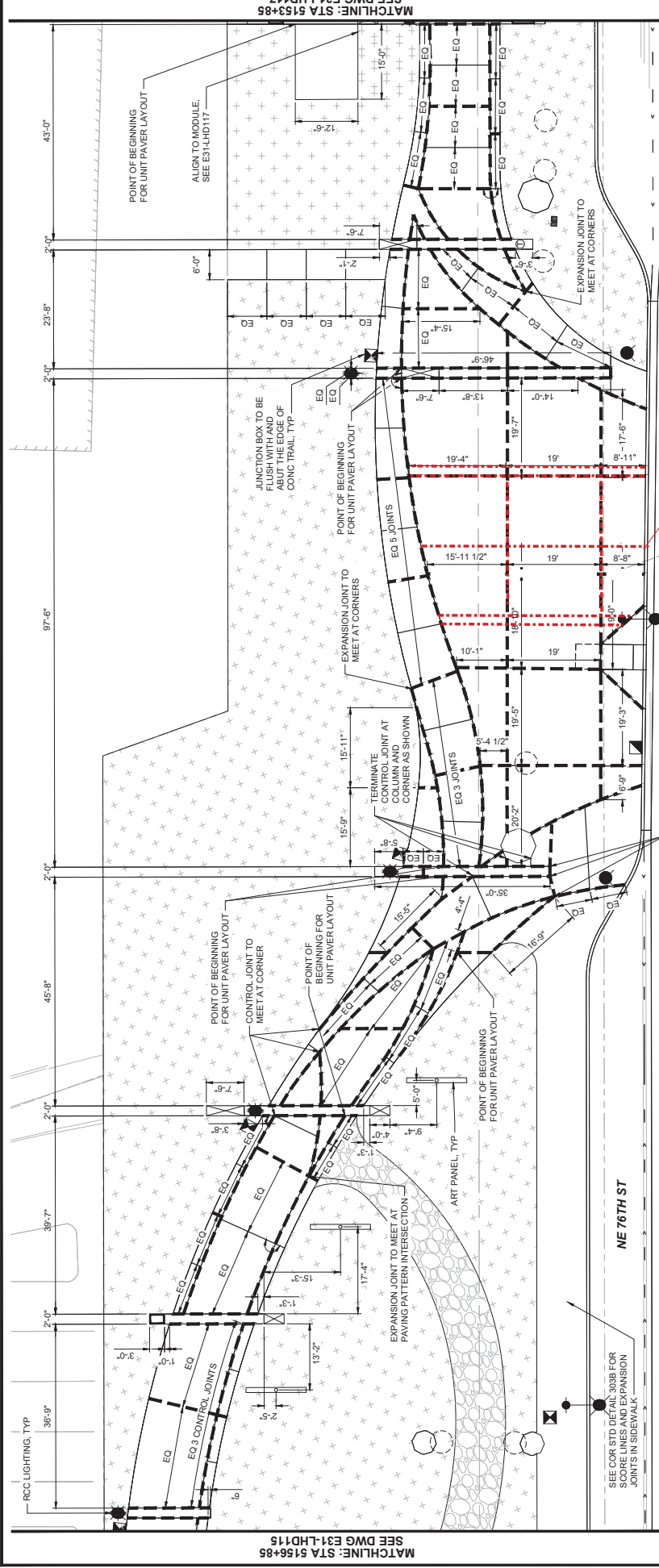
101/32022	JV	DK	JV	NDC-00871	DESIGNED BY:
104/20231	JV	DK	JV	NDC-00883	J. YONG
104/20231	JV	DK	JV	NDC-00883	M. WALTON
08/18/2021	JV	DK	JV	ISSUED FOR CONSTRUCTION	DESIGNED BY:
	NSN	DK	APP	REVISION	J. YONG

SCALE:	1" = 20'
FILENAME:	R200-E31-LPP103
CONTRACT NO.:	RTA/CN 0148-18
DATE:	08/18/2021

DESIGN PACKAGE:	DTS	D
PERMIT INFORMATION:		R

<b>DOWNTOWN REDMOND LINK EXTENSION</b> <b>CONTRACT R200</b> REDMOND TECH STATION TO DOWNTOWN REDMOND DOWNTOWN REDMOND STATION - LANDSCAPE PLANTING PLAN 3 OF 3	DRAWING NO.: <b>E31-LPP-003</b>
	FACILITY ID: E31 SHEET NO.: 448 REV.: B



Centered at 14 feet

1  
E31-LHD103

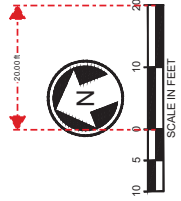
LEGEND

- 2 E31-LHD101
- 3 E31-LHD101
- EXPANSION JOINT
- CONTROL JOINT

NOTES:

1. SEE CIVIL GRADING SHEETS L80-CGP361 THROUGH L80-CGP365 AND GRADING POINT TABLES ON SHEETS L80-CGP361, L80-CGP365, L80-CGP362 AND L80-CGP363 FOR HORIZONTAL AND VERTICAL CURVING DATA.
2. SEE SHEETS E31-LHD101 THROUGH E31-LHD103 FOR CONCRETE FINISHES, LOCATION OF CONCRETE UNIT PAVERS, MODULES, SEATWALLS, BENCHWALLS, AND BENCH.
3. FOR JOINTS IN THE RCC TRAIL, INSTALL ALL CONTROL AND EXPANSION JOINTS PERPENDICULAR TO ARC AS SHOWN.
4. PROVIDE EXPANSION JOINTS WHERE SHOWN ON PLANS AND AT ALL TRANSITIONS TO BUILDING WALLS, SITE WALLS, SEAT WALLS, AND CURBS. EXPANSION JOINTS SHALL CONTINUE UNDER PAVERS.
5. ALL JOINT DIMS TAKEN ALONG CENTERLINE OF TRAIL OR BAND.

5 FDC-00398  
6 NDC-00083

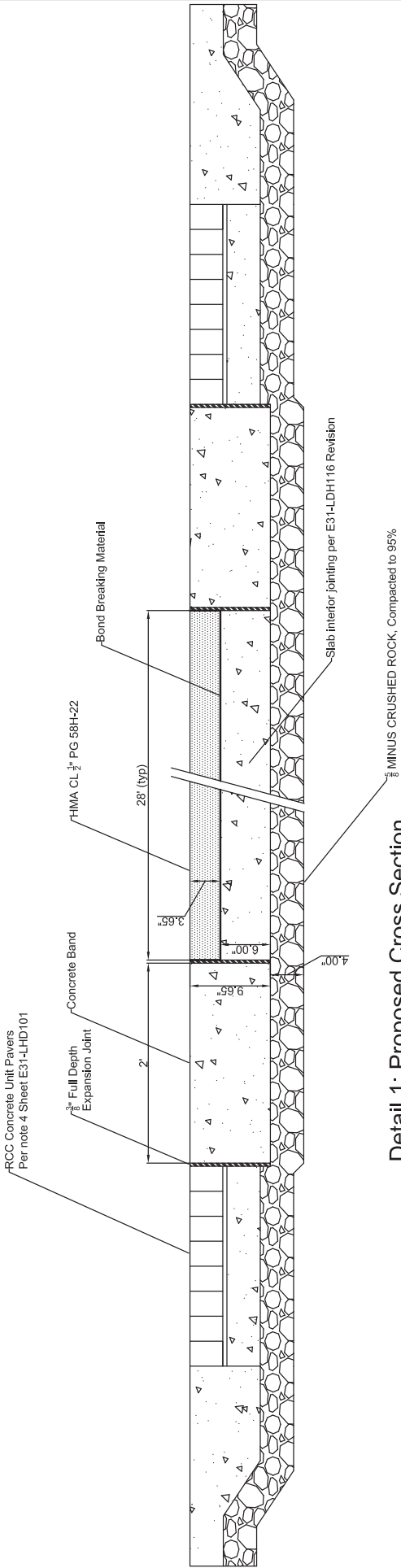


DATE: 08/18/2021  
BY: J. VONG  
CHECKED BY: D. KOONTS  
APPROVED BY: J. VONG

DESIGNED BY: J. VONG	SCALE: 1" = 10'	DESIGN: PALM BEACH	DTS: PERMIT INFORMATION	DRAWING NO: E31-LHD116
DRAWN BY: M. WALTON	FILENAME: R200-E31-LHD116	CONTRACTING: RTACN0148-18	CITY OF REDMOND ISSUED FOR PERMIT	CONTRACT R200
CHECKED BY: D. KOONTS	DATE: 08/18/2021	LINE IS 1" AT FULL SCALE	REDMOND TECH STATION TO DOWNTOWN REDMOND	FACILITY ID: E31
APPROVED BY: J. VONG	REVIEWED BY: R. GLEFFE	SUBMITTED BY: J. SCHEITLER	DOWNTOWN REDMOND STATION - LANDSCAPE URBAN DESIGN DETAILS	SHEET NO.: 440
DATE: 08/18/2021	BY: J. VONG	CHECKED BY: D. KOONTS	ISSUED FOR CONSTRUCTION	REV: B
DATE: 08/18/2021	BY: J. VONG	CHECKED BY: D. KOONTS	APPROVED BY: J. VONG	REV: B



DOWNTOWN REDMOND LINK EXTENSION  
CONTRACT R200  
REDMOND TECH STATION TO DOWNTOWN REDMOND  
DOWNTOWN REDMOND STATION - LANDSCAPE  
URBAN DESIGN DETAILS  
16 OF 18



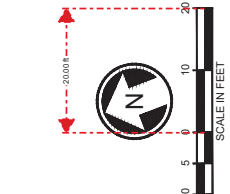
Detail 1: Proposed Cross Section





Temporary  
irrigation main  
behind RCC Trail

[illegible]

[illegible]

1. SEE THE GRADING SHEETS L90-CP086H THROUGH L90-CP084S AND L90-CP087 FOR THE FINISH GRADE OF THE EXISTING PAVING SURFACE.
2. SEE SHEET E3-LHP1010 THROUGH E3-LHP1013 FOR CONCRETE FINISHES, LOCATION OF CONCRETE UNIT PAVERS, MODULES SEATWALLS BENCHWALLS AND BENCH.
3. FOR JOINTS IN THE RCG TRAIL INSTALL A FULL CONTROL AND EXPANSION JOINT AT EACH END OF THE RCG TRAIL.
4. TRANSITION TO BUILDING WALLS, SITE WALLS, SEAT WALLS AND CURBS WITH AN INTERLOCKED JOINT.
5. ALL JOINT SHIMS TAKEN FROM CENTERLINE OF TRAIL OR BAND

									
DESIGNED BY: J. WONG		CHECKED BY: M. WALTON		D. KOONTS		APPROVED BY: J. VONG		SCALE: 1" = 10'	
A. 1/02/2022 JV AL JV FQC-00368		B. 1/04/2021 JV DK JV NDC-00063		C. 08/19/2021 JV DK JV ISSUED FOR CONSTRUCTION		D. 08/19/2021 JV DK JV ISSUED FOR PERMIT		CONTRACT NO.: E20EA-E3-LHD0116	
DATE		DATE		DATE		DATE		CITY OF REDMOND	
REVISION		REVISION		REVISION		REVISION		ISSUED FOR PERMIT	
DATE		DATE		DATE		DATE		REGION: PACKAGE:	
DATE		DATE		DATE		DATE		PERMIT INFORMATION	
DATE		DATE		DATE		DATE		CONTRACT R200	
DATE		DATE		DATE		DATE		REDMOND TECH STATION TO DOWNTOWN REDMOND	
DATE		DATE		DATE		DATE		DOWNTOWN REDMOND STATION - LANDSCAPE	
DATE		DATE		DATE		DATE		URBAN DESIGN DETAILS	
DATE		DATE		DATE		DATE		16 OF 16	
DATE		DATE		DATE		DATE		SHEET NO.: 440	
DATE		DATE		DATE		DATE		REV: E31	
DATE		DATE		DATE		DATE		DRAWING NO.: E31-LHD116	
DATE		DATE		DATE		DATE		DOWNTOWN REDMOND LINK EXTENSION	